

## COFFS JETTY STRIP STRUCTURE PLAN



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## 1. OVERVIEW

Harbour Drive, particularly between Edinburgh Street and Marina Drive, is an important 'Gateway' into the Jetty and Marina Precinct.

Dining and entertainment establishments on the southern side of Harbour Drive, from Camperdown Street to North Coast Railway line, offer a unique experience in Coffs Harbour. This part of Harbour Drive is often referred to as 'Jetty Strip' (hereafter also referred to as the Jetty Strip Precinct). The remainder of the Harbour Drive corridor, however, is dominated by traffic and parking functions, which do not contribute to the gateway experience.

There is currently considerable pressure from businesses and landholders in the Jetty Strip area to improve the function and appeal of the location, including pedestrian circulation, amenity, traffic flow and parking. Thoughtful and effective stakeholder engagement and design is required to realise long-term benefits for the Jetty Strip and the community.

This project has been initiated by Council's adoption of the Local Growth Management Strategy and development interests in several key sites on Harbour Drive and Orlando Street.

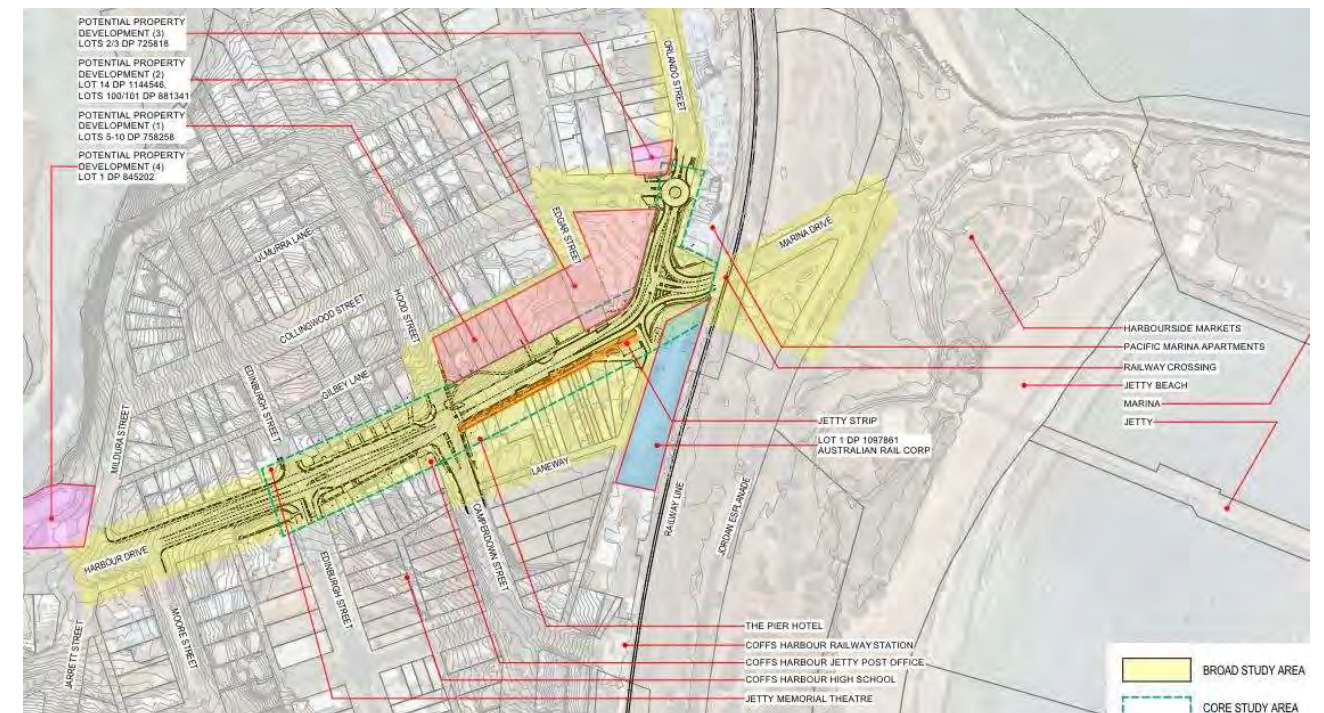
### 1.1 The Study Area

The core Study Area is Harbour Drive and Orlando Street between Edinburgh Street and Collingwood Street.

The Study Area must also consider key connections with and influences on the core Study Area, including:

- Road approaches along Harbour Drive, Marina Drive and Orlando Street;
- Side streets, particularly Collingwood Street, Edgar Street, Camperdown Street and Hood Street;
- Key development sites on the northern side of Harbour Drive and western side of Orlando Street respectively;
- Pedestrian connections with the Jetty and Marina Precinct and Coffs Harbour High School.

The Study Area is illustrated in **Figure 1**.



**Figure 1 – The Study Area**



**Important first view of sea down Harbour Drive from Edinburgh Street – This marks the start of Core Study Area**



## 1.2 Intent of the Coffs Jetty Strip Structure Plan

The intent of the Coffs Jetty Strip Structure Plan is to work with community stakeholders in developing a preferred direction for the spatial configuration of streets and open space in the Study Area, and provide suggestions for future streetscape character and materials – which can be further developed in future detailed design phases.

A project needs to adopt a place-based design approach with focus on:

- Streetscape and open space improvements;
- Pedestrian and cycleway connectivity;
- Enhancing the 'Gateway' connection into the Jetty and Marina Precinct;
- Maintaining traffic and parking functions;
- Improving amenity and safety generally;
- Recognising the areas unique history; and
- Encouraging future development that will activate the streetscape and contribute to the Precinct generally.

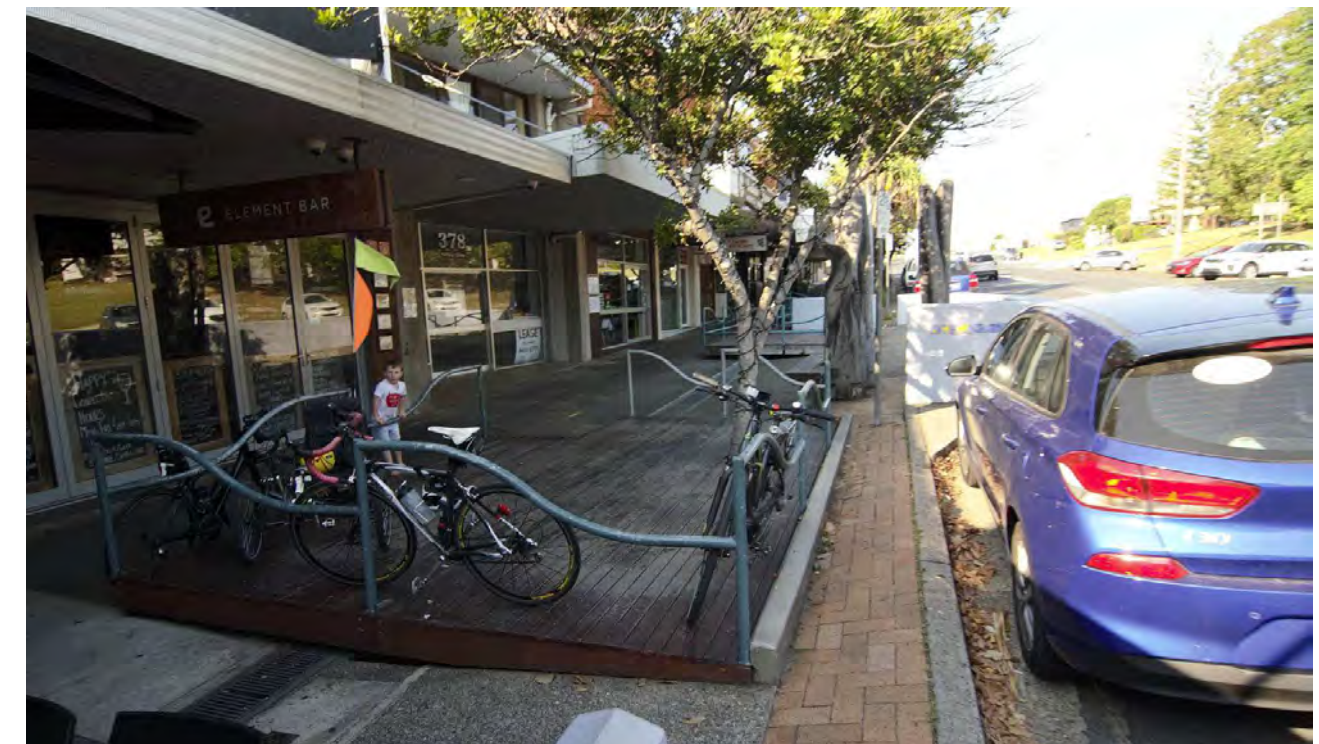


Harbour Drive is dominated by traffic and car parking. Whilst this is an important function it needs to be balanced against the needs of pedestrian circulation and amenity

## 1.3 An Overall Vision for the Jetty Strip

An overall vision for the Jetty Strip, as developed through the engagement process is:

*To retain and build on the unique character of the Jetty Strip by providing an attractive and memorable street environment which reflects its heritage and its broader roles as an important tourist and entertainment destination and gateway into the Jetty and Marina Precinct.*



Entertainment and dining along the Jetty Strip has long provided memorable experiences for Coffs Harbour locals and visitors – but the interface with Harbour Drive is uncomfortable and the facilities are worn and tired looking.



## 2. SITE ANALYSIS

### 2.1 Site Context and Broad Circulation Network

The Jetty Strip is strategically located at the end of Harbour Drive, the main movement thoroughfare between Coffs Harbour Central Business District and the Coffs Jetty and Marina Precinct.

Owing largely to it being the one of the few crossings of the North Coast Railway, Harbour Drive and Orlando Street, into Marina Drive, presents a 'choke point' and the only effective entry into the Jetty and Marina Precinct – and hence it is an important 'Gateway'.

The Coffs Jetty and Marina Precinct has seen road, parking and open space improvements in recent years which has enhanced both its function and appeal for locals and tourists alike. The NSW Government have large landholdings along the North Coast Rail corridor and in the Jetty and Marina Precinct. In 2018 a preliminary concept plan for the Jetty and Marina Precinct was publicly released which illustrates potentially significant, residential and open space development. Whilst these recent and potential future improvements are applauded, they have and will continue to result in increasing traffic pressures in the area generally.

Careful consideration needs to be given to the impacts of increased traffic generation on the broader traffic circulation network, including Harbour Drive and Orlando Street. Whilst the assessment of these broad traffic impacts is largely beyond the scope of this study, the following is noted:

1. Harbour Drive and Orlando Street currently carry in excess of 10,000 vehicles per day.
2. Future development in the Coffs Jetty and Marina Precinct has the potential to significantly increase traffic volumes in Marina Drive, Harbour Drive and Orlando Street.
3. The potential for vehicles to find alternative routes around the Jetty Strip into the future via the local road network is limited, in large part owing to unsuitable road grades (particularly in the case of Edinburgh and Hood Streets) and disruption to existing residential amenity.

It is for the above reasons that it is strongly recommended that an alternative traffic route into the Coffs Jetty and Marina Precinct be identified, and any future development in the Precinct be contingent on its implementation – It is understood that an alternative route via Hogbin Drive, Howard Street and a bridge over the North Coast Rail line is currently being considered by Council and State Government. This initiative is supported.

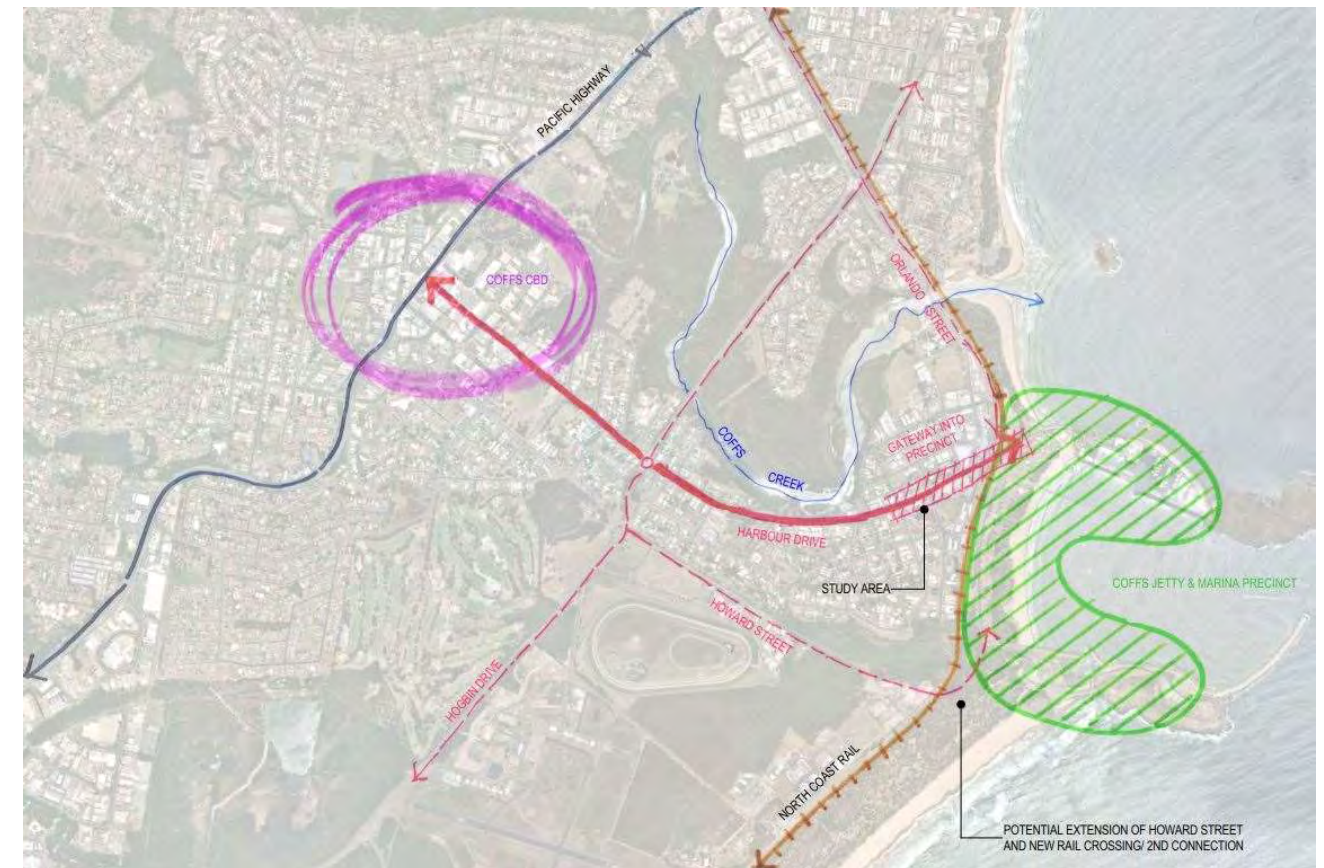


Figure 2 – Site Context

### 2.2 Site History

It is not the intent of this project, nor is it necessary, to undertake a detailed historical analysis of the Study Area. However, it is important to recognise the significance of the Jetty Precinct's history in developing proposals that pay homage to its heritage.

The Study Area has significant European Heritage.

A previous study, Jetty Strip Streetscape Concept Plan (Coffs Harbour City Council, 2016) provides a summary of the history of the Jetty Strip. Salient points of this summary are:

- Coffs Harbour's early history is intimately linked to the development of coastal shipping;
- Coffs Harbour Jetty was completed in 1892;
- In 1892 and 1893 a more direct vehicle route to the Jetty was established and many adjacent streets were named after ships, admirals and naval battles (eg. Camperdown);
- From 1905 private timber milling interests built tram lines to bring cut logs to the Jetty;



- A postal receiving station was opened in 1905 and upgraded to a post office in 1906;
- Buildings were established on the Jetty Strip following the construction of the Jetty, including the Pier Hotel (1905), the City Bank (1907) and Andertons Store (1908);
- The Pier Hotel was destroyed by fire in 1914 and rebuilt on the current site. It was altered in 1937;
- The original post office was also destroyed in the (same) 1914 fire. It was rebuilt on its current site in 1926;
- In 1912 work on the breakwater commenced and in 1914 the headland quarry was opened.

The Jetty structure itself, being the connection of Coffs Harbour to the sea, has been the primary impetus for development in the Precinct. The Jetty's construction, utilising industrial scale materials including large sectioned timbers, provides a strong thematic base for the identification of suitable streetscape materials, including paving and furniture.



**Coffs Jetty – A significant heritage item and an important material reference for street upgrade works**

The project study area is part of the Gumbayniggirr Nation. Gumbayniggirr Nation is one of the largest nations in NSW, stretching from Nambucca River in the south to around the Clarence River in the north, and the Great Dividing Range in the west.

Yandaarra, Shifting Camp Together – A Guide for Aboriginal Cultural Awareness and Engagement (Coffs Harbour City Council, 2019) is an important reference developed by Council through the local aboriginal community. This document provides guidance on how local aboriginal land groups should be engaged. In the first instance the document recommends contacting Council's Aboriginal Community Planning Specialist and the Yandaarra Aboriginal Advisory Committee.

As part of the next phases of design for the project Council will consult with the indigenous community in accordance with Yandaarra, Shifting Camp Together.



### 3. CURRENT JETTY STRIP EXPERIENCE - GENERAL IMPRESSIONS

#### 3.1 First Impressions

The following general impressions of the Jetty Strip Precinct are in part the observations of the Consultant Team as visitors to the site and as urban design professionals, and in part the observations and experiences of Council staff and community stakeholders (stemming from the engagement process).

- The Jetty Strip on the southern side of Harbour Drive serves an important food and entertainment function. Whilst the Strip has long provided memorable dining and entertainment experiences, on-street facilities appear old and worn and the area is in need of refurbishment;
- The Jetty Strip Precinct is dominated by traffic and parking functions;
- With the exception of two marked pedestrian crossings, general pedestrian crossing of Harbour Drive is difficult owing to large traffic volumes, traffic dominated functions and wide road crossing distances;
- Existing development on the northern side of Harbour Drive provides minimal activation and interest to the street due to building type (government offices), building age and large building set-backs;
- With the exception of the fine grain nature and interest of the Jetty Strip, the Jetty Strip Precinct, including neighbouring development, provides little amenity value and is not memorable;
- Parking generally in the Precinct is difficult and there is anecdotal evidence that patrons of restaurants on the Strip are utilising the off-street carparking areas of neighbouring development (including the Jetty Village and Pier Hotel);
- The configuration of the intersection of Harbour Drive, Orlando Street and Marina Drive biases Harbour Drive/Orlando Street through traffic movement at the expense of providing a more direct route/axis into the important Jetty and Marina Precinct;
- The pedestrian thoroughfare recently constructed on the Jetty axis ends abruptly at the edge of NSW Property land associated with the North Coast Railway, leaving an awkward and weak pedestrian link to the Jetty Strip via the existing rail crossing pedestrian access;
- Existing pedestrian footpath connections from the Jetty Foreshore to the Jetty Strip are generally weak including, particularly, the link across the top of Harbour Drive to the laneway and start of the (eastern side of) the Jetty Strip;
- Pedestrian access along the road side of outdoor dining areas on the southern side of Harbour Drive is very narrow and ineffective in some sections;
- The large Jetty Village development and government office buildings (on Development Sites 1 and 2) do little to contribute to street character and general amenity. Their redevelopment will be key to realising improved public realm outcomes for the whole Precinct;
- Street configuration and levels in the general vicinity of the Pier Hotel, including the intersection of Camperdown Street and Harbour Drive, present particularly difficult issues in terms of pedestrian crossing, footpath grades and amenity generally;

- The mid-block crossing in Harbour Drive is well used and located;
- The marked crossing adjacent the Pier Hotel is well used, including by Coffs Harbour High School students;
- Existing on-street parking in Harbour Drive is valuable and well used.



**The new pedestrian thoroughfare on the Jetty axis is a welcome addition to the Jetty and Marina open space network – but it terminates abruptly and does not link well to the Jetty Strip.**



### 3.2 Road and Parking Functions

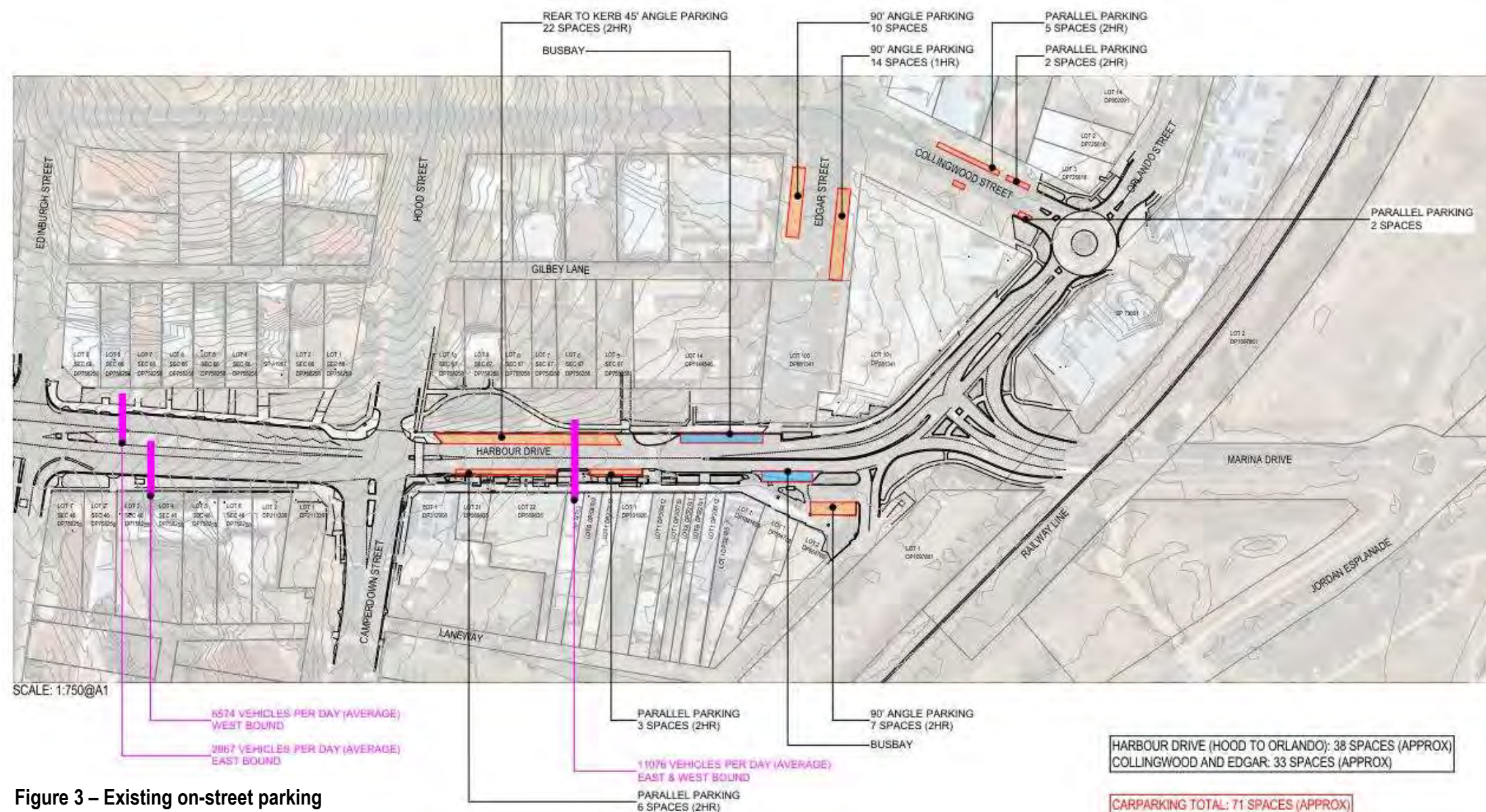
Harbour Drive and Orlando Street connect Coffs Harbour City Centre to residential and commercial areas on the eastern side of Hogbin Drive and the Coffs Jetty and Marina Precinct. The road in the core Study Area carries in excess of 10,000 vehicles per day. With the exception of a local road bridge crossing at the end of Camperdown Street, Harbour Drive provides the only access into the Jetty and Marina Precinct, via an at-grade rail crossing.

One of the key desirable design outcomes identified at the outset of this project was the straightening/realignment of Harbour Drive – specifically at the Harbour Drive/ Orlando Street/ Marina Drive intersection – to provide a more direct connection into the Jetty and Marina Precinct and to reinforce Harbour Drive and Marina Drive's role as a major 'Gateway' function. To inform the traffic analysis undertaken as part of this intersection reconfiguration ground surveys (traffic counts) and preliminary traffic modelling were undertaken.

In summary, this analysis found that the Harbour Drive/ Orlando Street/ Marina Drive could be realigned, and simplified without causing significant impacts to the traffic network, although the timing of the work, and whether traffic signalisation is required as part of it (particularly to meet pedestrian crossing demands) needs to be further investigated as part of the next phases of design.

As illustrated in **Figure 3**, there are approximately 38 on-street car spaces in Harbour Drive, including seven (7) spaces in a small (off-street) parking area at the eastern end of the Jetty Strip, and approximately 33 on-street carparking spaces in Collingwood and Edgar Streets.

There are two bus stops on Harbour Drive, one on either side of the street in close proximity to existing retail and commercial areas.





### 3.3 Current Landuses

The Study Area's development history stems back to the early 1900's, immediately following construction of the Jetty. A number of older buildings remain, including the Pier Hotel and the Post Office, though much of the development that does exist is of mixed age, two to three storeys and generally of fair condition.

Older government office buildings, including the NSW Public Works Building and the former State Forestry building, are located on the northern side of Harbour Drive and are generously set back from the road. Jetty Village is an expansive three storey complex located on the corner of Harbour Drive and Orlando Street. Whilst existing development on the northern side of the road is older and run-down, and provides almost no activation at street level, it is contained in two large ownership parcels – And this offers good prospects for redevelopment.

Conversely, property ownership on the southern side of Harbour Drive is fragmented and offers limited opportunity for consolidation and redevelopment. Fortunately, the buildings on these properties provide good existing activation of Harbour Drive – by way of, predominately, the line of predominately dining and entertainment businesses constituting the Jetty Strip.



**Existing development on the northern side of Harbour Drive is older, in limited ownership and provides good opportunity for redevelopment. This in turn offers good potential for private/public partnerships.**



### 3.4 Coffs Harbour Local Environmental Plan 2013

The following planning provisions set out in Coffs Harbour Local Environmental Plan (Coffs Harbour LEP) apply to the Study Area:

- Land Zoning – As illustrated on the zone plan extract the majority of the Study Area is zoned B4-Mixed Use Zone. Development permissible with consent in this zone is as follows:
  - “Zone B4 Mixed Use
    - 1 Objectives of zone
      - To provide a mixture of compatible land uses.
      - To integrate suitable business, office, residential, retail and other development in accessible locations so as to maximise public transport patronage and encourage walking and cycling.
      - To facilitate the development of a mix of local-scale facilities and services that do not detract from the core commercial functions of the Coffs Harbour central business district.
      - To ensure that new commercial buildings make a positive contribution to the streetscape and contribute to a safe public domain.
    - 2 Permitted without consent
 

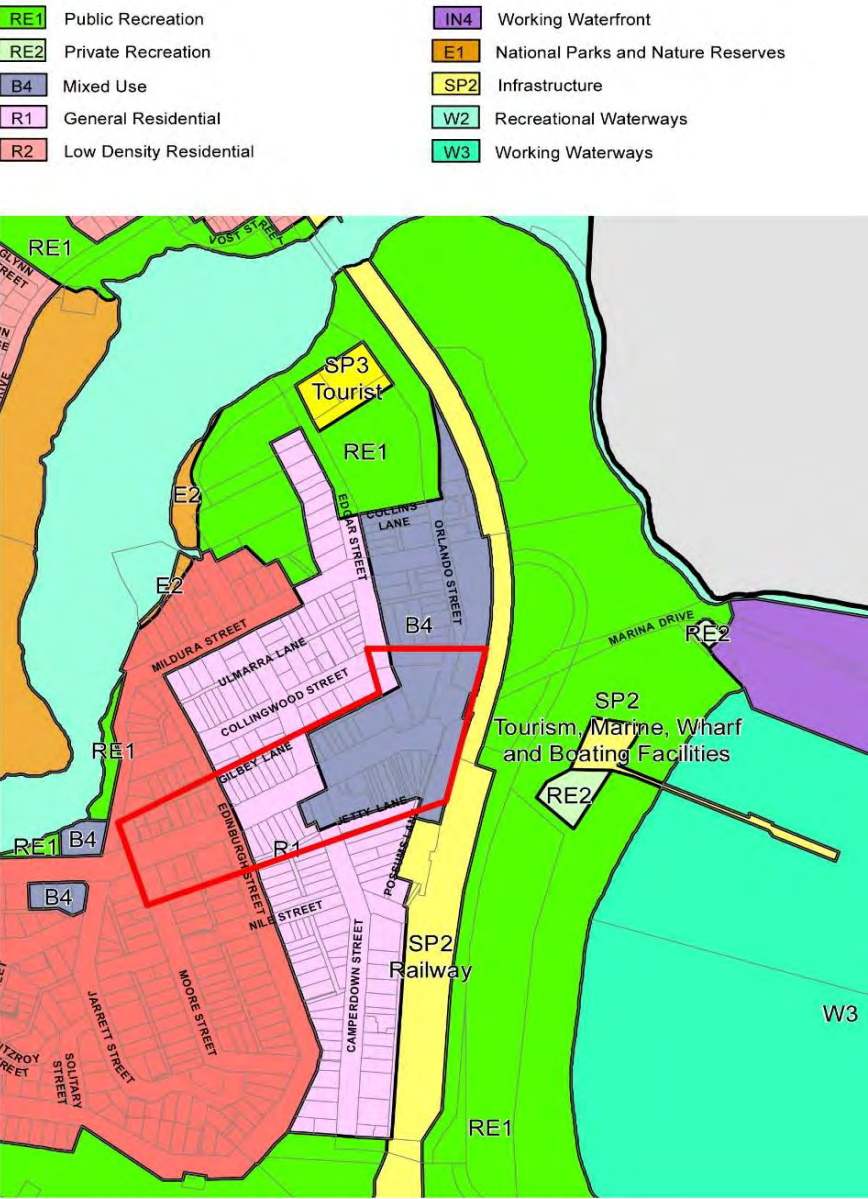
*Building identification signs; Home-based child-care; Home businesses; Home occupations*
    - 3 Permitted with consent
 

*Boarding houses; Centre-based child care facilities; Commercial premises; Community facilities; Educational establishments; Entertainment facilities; Function centres; Home industries; Hostels; Hotel or motel accommodation; Information and education facilities; Medical centres; Multi dwelling housing; Oyster aquaculture; Passenger transport facilities; Recreation facilities (indoor); Registered clubs; Residential flat buildings; Respite day care centres; Restricted premises; Roads; Seniors housing; Shop top housing; Tank-based aquaculture; Any other development not specified in item 2 or 4*
    - 4 Prohibited
 

*Agriculture; Air transport facilities; Air Strips; Animal boarding or training establishments; Biosolids treatment facilities; Boat building and repair facilities; Boat launching ramps; Boat sheds; Camping grounds; Caravan parks; Cemeteries; Charter and tourism boating facilities; Correctional centres; Crematoria; Depots; Eco-tourist facilities; Electricity generating works; Exhibition homes; Exhibition villages; Farm buildings; Farm stay accommodation; Forestry; Freight transport facilities; Heavy industrial storage establishments; Helipads; Highway service centres; Industrial retail outlets; Industrial training facilities; Industries; Jetties; Marinas; Mooring pens; Moorings; Mortuaries; Pond-based aquaculture; Recreation facilities (major); Residential accommodation; Resource recovery facilities; Rural industries; Sewage treatment plants; Storage premises; Transport depots; Truck depots; Vehicle body repair workshops; Vehicle repair stations; Warehouse or distribution centres; Waste disposal facilities; Water recreation structures; Water recycling facilities; Water treatment facilities; Wharf or boating facilities.”*

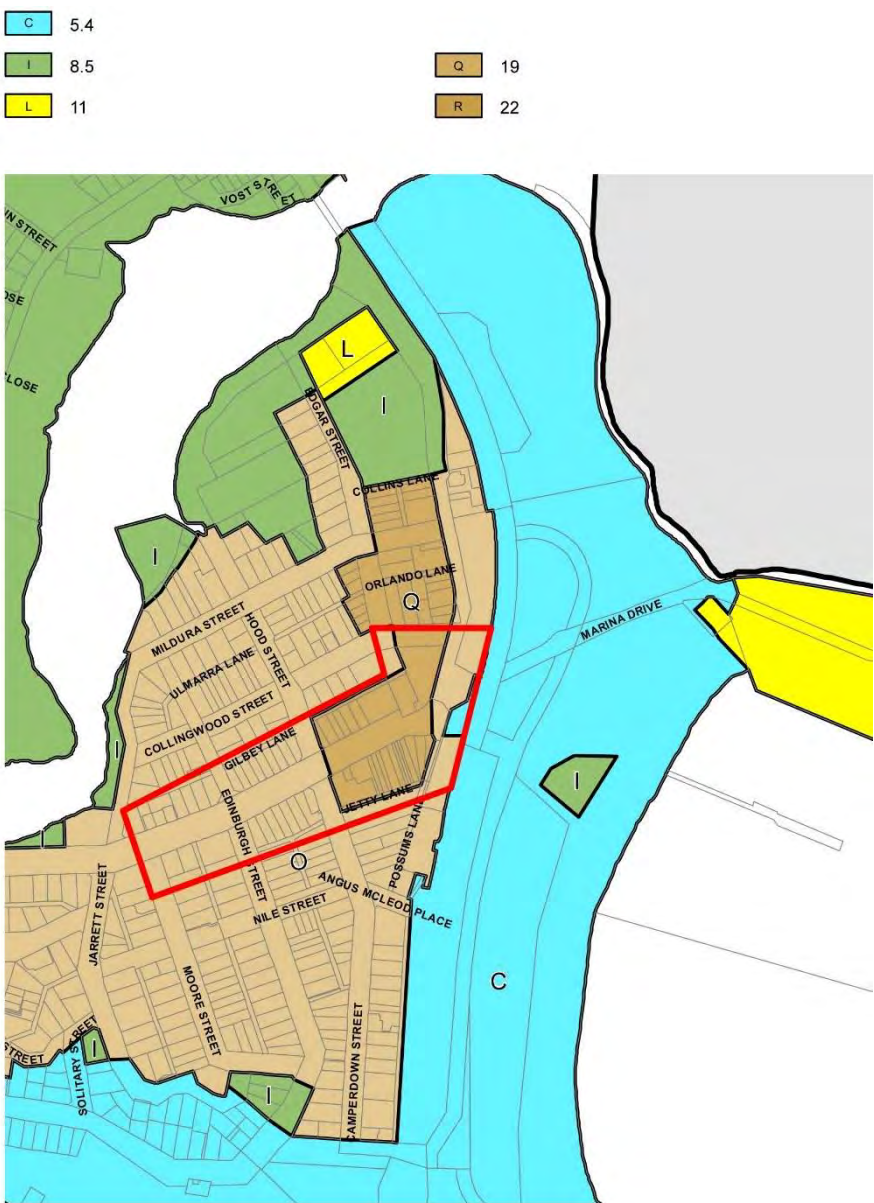


**Land Zoning** –The Study Area contains a mix of B4-Mixed Use, R1-General Residential and R2-Low Density Residential.



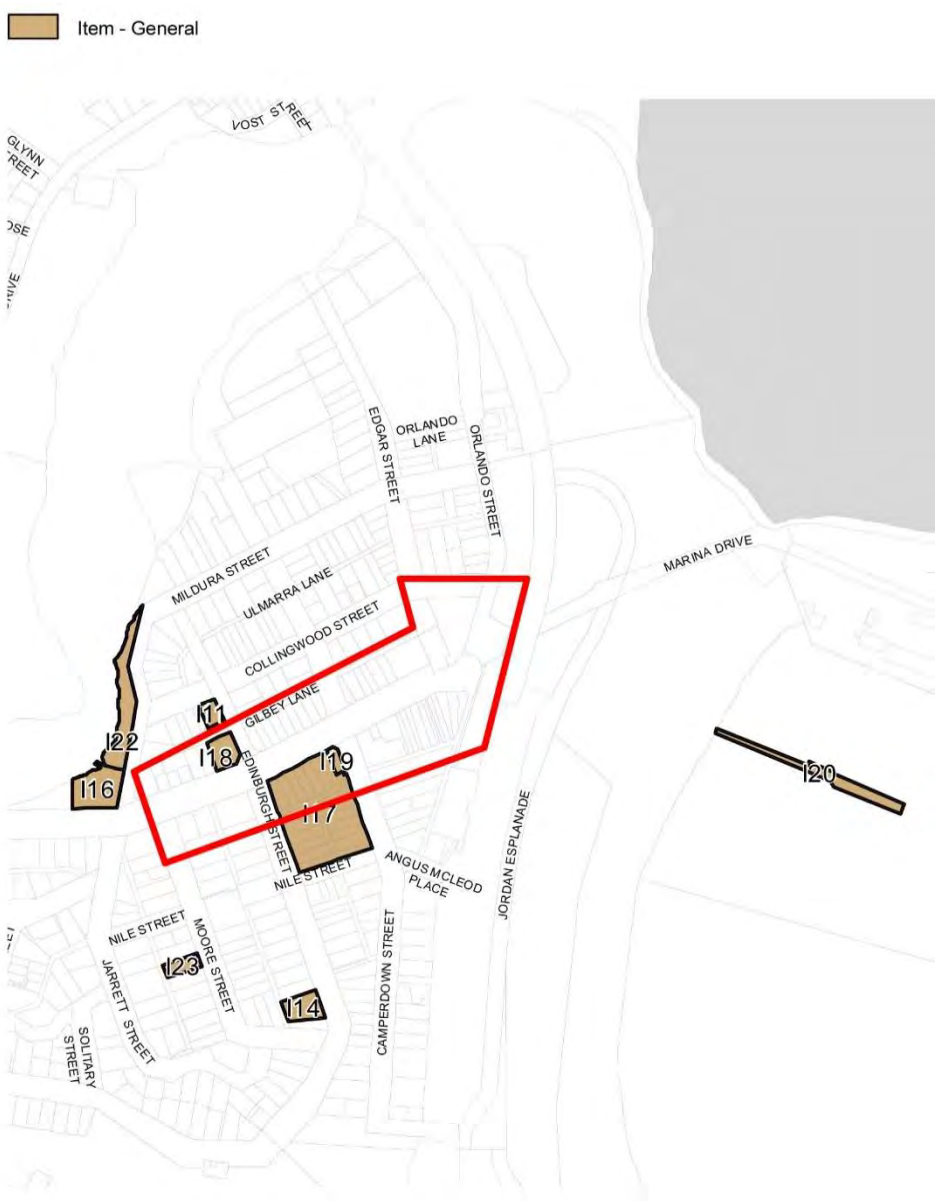
Source: Extract from Coffs Harbour LEP 2013–Land Zoning Map

**Height of Buildings** – As illustrated on the Maximum Building Height Plan, the majority of the study has a maximum building height of 19m.



Source: Extract from Coffs Harbour LEP 2013–Building Height Map

**Heritage Items** – There are a number of Heritage items that relate directly to the Study Area. These items include the Jetty, Jetty Theatre, Coffs Harbour High School and Post Office.



Source: Extract from Coffs Harbour LEP 2013–Heritage Map



### 3.5 Coffs Harbour City Council DCP 2015 (DCP)

The DCP has a number of relevant control objects and requirements for the Coffs Harbour Jetty Business Precinct. Key objectives for the Precinct are as follows:

#### Design

- To strengthen the area as a tourist precinct with a distinctive character.
- To promote development which complements the 'Jetty Strip'.
- To preserve sight corridors and views to the foreshore area.

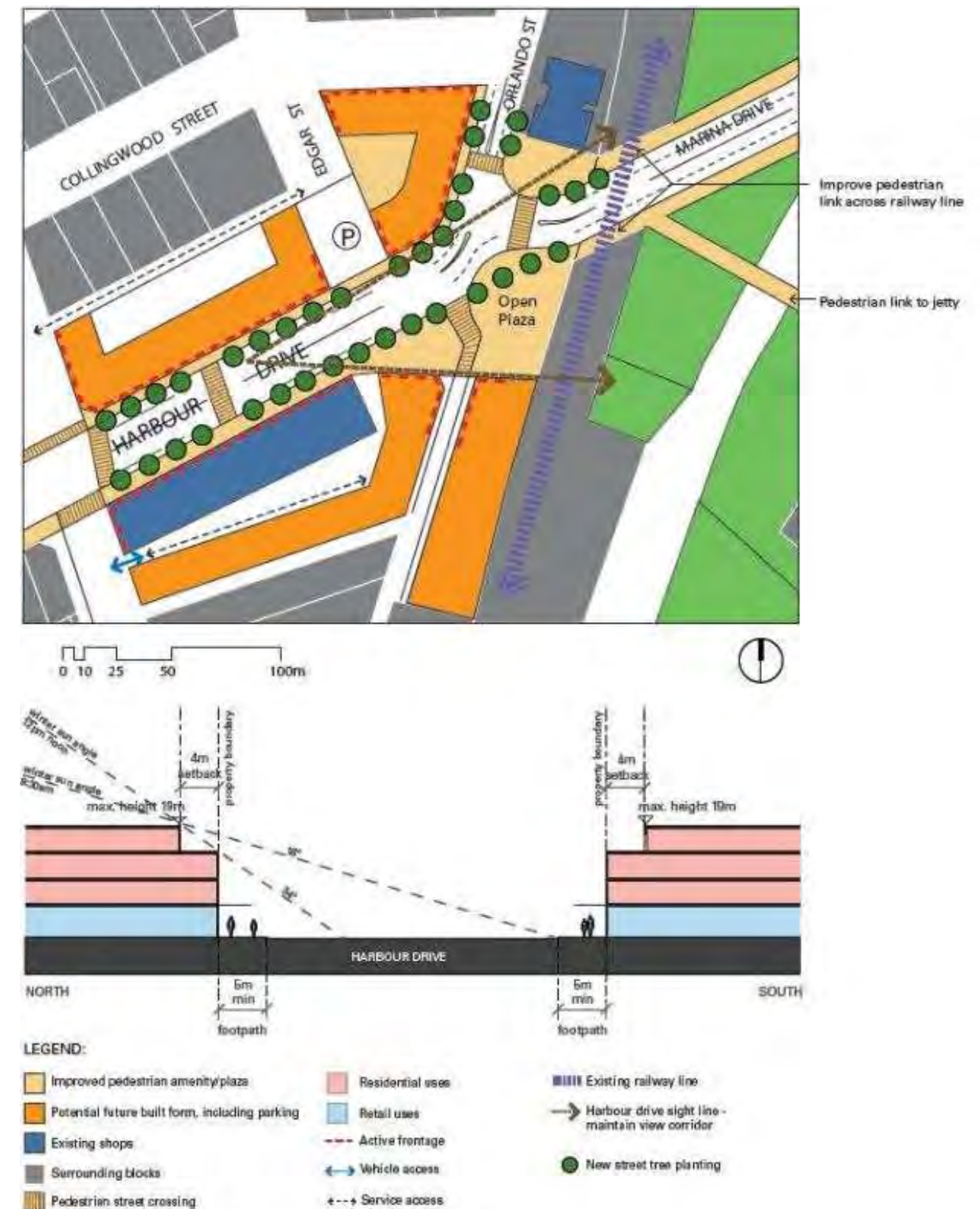
#### Setbacks

- To ensure that building frontages have consistent alignment and visual appearance.
- To achieve active street frontages.
- To preserve sight corridors and views to the foreshore area.

#### Access and parking

- To ensure that appropriate access, vehicular manoeuvring area and on site car parking is provided for development.

These objectives, and specific requirements relating to them need to be considered in the development of proposals for the Jetty Precinct.



Source: Extract from Coffs Harbour City Council DCP 2015



## 3.6 Key Development Opportunities/ Objectives

Current underutilised land uses and statutory planning provisions (stipulated in Coffs Harbour LEP) provide a significant opportunity for redevelopment in the Jetty Strip Precinct, particularly on the northern side of Harbour Drive and western side of Orlando Street.

A number of potential development sites have been identified, as follows:

- Site #1 – Lots 5-10 DP 758258
- Site #2 – Lot 14 DP 1144546 & Lots 100-101 DP 881341
- Site #3 – Lots 2-3 DP 725818
- Site #4 – Lot 1 DP 845202

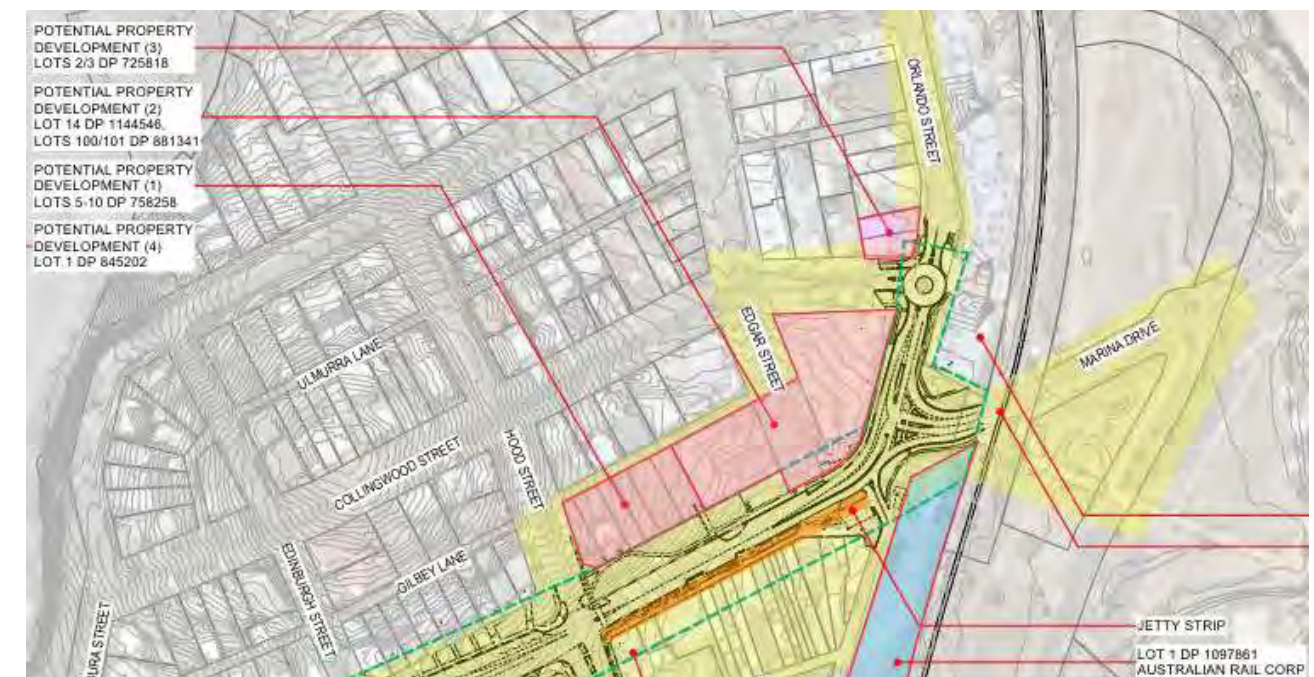
The road reserve in Harbour Drive and Orlando Street is variable in width, in part 40 metres (on Site #1 and Site #3 frontage) and in parts as narrow as 25m (Site #1 frontage to Orlando Street).

The future Harbour Drive configuration will require a consistent road reservation width; hence road closure/property boundary adjustment is necessary. This, and development opportunities favouring key sites along Harbour Drive, provides the impetus for public/private landholder co-operation. Key design objectives for the Study Area that should underpin any road closure/property boundary adjustment negotiations and agreements are:

- All future buildings should address Harbour Drive, Orlando Street and Collingwood Street, including maximizing ground floor activation. Small (fine) scale retail and food outlets addressing the street at ground level are encouraged;
- Limited vehicle access off Harbour Drive and Orlando Street – in relation to Site #1, the existing left-in (only) access off Harbour Drive will likely need to be retained;
- All new development should provide appropriate levels of on-site carparking (to meet current Coffs Harbour DCP requirements); and,
- Buildings fronting Harbour Drive should minimize overshadowing impacts on restaurants and outdoor dining areas on the southern side of Harbour Drive.

### NSW Property land – Lot 1 DP1097861

- It is understood that the future development of this vacant land parcel is currently being considered by the NSW State Government.
- This land parcel will be critical in realising broader objectives for the Precinct, that being to provide a strong open space and pedestrian link from the Jetty Strip to the Jetty and Marina Precinct, and providing off-street parking to service current shortfalls in the Jetty Strip (particularly).
- It is recommended that Council and State Government continue to work together towards realising this objective.



**Land parcels with significant opportunity for development**



4. BROAD CONCEPTS – BUILDING TO SOLUTIONS

*Note – This section of the report should be read in conjunction with the drawing package contained in Appendix A*

4.1 What Makes a Vibrant Main Street?

When a Main Street works it is vibrant and memorable?

A great example of a Main Street that works is First Avenue in Sawtell. In this Main Street the (at times) competing functions of vehicle and pedestrian circulation and parking, amenity and activation are well balanced.

A good visual test of the success of a balanced Main Street is when all functions work well, and no particular function appears to dominate.

Getting the balance right is critical in achieving a vibrant and memorable place.

People Movement	<ul style="list-style-type: none"><li>• Vehicle circulation</li><li>• Vehicle parking</li><li>• Pedestrian circulation (including cyclists and mobility scooters)</li></ul>	Making Memorable Places
Security	<ul style="list-style-type: none"><li>• Safety</li><li>• Convenience</li></ul>	
Amenity	<ul style="list-style-type: none"><li>• Sensory stimulation</li><li>• Comfort</li><li>• Uniqueness (of place)</li></ul>	
Activation	<ul style="list-style-type: none"><li>• Viable business</li><li>• Food, entertainment and retail experiences</li><li>• Meeting opportunities</li></ul>	



Harbour Drive is dominated by traffic functions and is not a balanced and memorable Main Street



First Avenue, Sawtell is an excellent Main Street that provides a good balance of all competing functions



## 4.2 Current Imbalances

There are a number of critical imbalances in the Study Area, and particularly Harbour Drive, that will need to be addressed if a vibrant and memorable Main Street is going to be achieved. These imbalances are:

- Pedestrian Crossing – Owing largely to high traffic volumes (in excess of 10,000 vehicles per day), Harbour Drive is difficult and uncomfortable to cross. In addition, the road and carparking carriageway is wide and forces pedestrians to cross approximately ten metres of bitumen without a break. This effectively divides one side of the street from the other and significantly erodes the sense of pedestrian/vehicle sharing that characterise successful Main Streets.
- Lack of Active Frontage – the northern side of Harbour Drive is virtually devoid of active building frontage.
- General lack of shared space definition – at present the street is dominated by road and parking functions and there is no definition of pedestrian activity zones, other than the two marked pedestrian crossings.
- Lack of Amenity – with the exception of the Jetty Strip and its associated outdoor dining areas there is a general lack of visual appeal and uniqueness in Harbour Drive and Orlando Street.
- Tired – much of the existing street and building infrastructure, including the outdoor dining areas associated with the Jetty Strip, appear worn and tired and are in need of refurbishment and/or replacement.

## 4.3 Straightening Harbour Drive

As discussed previously in **Section 3.2**, a key desirable design outcome identified at the outset of this project was the straightening/realignment of Harbour Drive – specifically at the Harbour Drive/ Orlando Street/ Marina Drive intersection – to provide a direct connection into the Jetty and Marina Precinct. Traffic analysis undertaken as part of the investigation of this intersection reconfiguration indicates that this straightening/realignment is possible and will not result in significant impacts to the traffic network (see also traffic analysis contained in **Appendix B**).

This intersection reconfiguration is considered integral to the success of the 'Boulevard' concept outlined below. In the opinion of the Consultant Team, the 'Boulevard' concept will be significantly weakened without this intersection reconfiguration, and likewise, and conversely, there is little point in reconfiguring the intersection if the 'Boulevard' concept is not implemented.

## 4.4 The 'Boulevard' Concept

The 'Boulevard' concept is put forward as a fundamental design concept that has the potential to balance competing Main Street functions. The 'Boulevard' concept is proposed for the core area of Harbour Drive, between Camperdown Street and Orlando Street.

The 'Boulevard' concept is illustrated in the Structure Plan drawing set (in **Appendix A**). It comprises the following key elements:

- A continuous at-grade median – this is the most important element of the Boulevard concept. A generous (3 metre wide) at-grade median effectively halves pedestrian crossing distances and necessitates the negotiation of traffic travelling from one direction (only) at a time. This effectively becomes a bridging mechanism for the street. Additionally, this at-grade median will also separate traffic carriageways and promote traffic (speed) calming. Importantly, in the instance of Harbour Drive, no u-turns across the pedestrian refuge are recommended to be permitted, by way of planting pots (as bollards). This approach is consistent with the majority of the length of this existing section of Harbour Drive (where u-turns are not currently permitted).



Recently installed at-grade median in Inverell – It assists pedestrian crossing, promotes traffic calming and provides an opportunity for significant tree planting.



- The continuous at-grade median also provides the potential for significant tree planting in the centre of the street. Using a well-designed system of continuous trenching, concrete side walls, structural tree root cells, porous paving (no-fines concrete), permeable paving, irrigation and drainage, a suitable (engineered) environment can be created to promote large tree growth whilst maintaining road pavement integrity.
- This system has been incorporated in other centres, including, most recently, Otho Street, in Inverell (see images). It is important to note that whilst u-turns across the median are permitted in Inverell (in a calmed traffic environment), this function is not recommended for Harbour Drive, owing largely to its different parking formats and higher through traffic volumes.



Recently installed at-grade median in Inverell – The detailing and construction of the median has to be done well – including, structural tree root cells, tree planting vaults and permeable paving and drainage.

- Wider footpaths on the southern side of the street – current footpaths and outdoor dining areas are contained in a variable width building-line to kerb-line dimension – of approximately 7 to 8 metres. It is proposed to widen this dimension to a consistent 9 metres and incorporate a continuous 2m (minimum) footpath to the back of the kerb. This will provide for improved pedestrian circulation and public realm amenity generally.
- Carriageways and on-street parking – two 7.2 metre road carriageway and parking corridors are proposed (separated by the at-grade median). The road carriageways are 3.5 metres wide and parallel parking is recommended to be marked at 2.3 metres width. This leaves 1.4 metre width for vehicle manoeuvring space and/or the possible inclusion of an on-road cycleway. When viewed against Australian Standards (AS 2890.5 – 1993 On-Street Carparking), 7.2 metres is a generous width (calculated as a 6.6 metre minimum standard), and hence should be considered to be a maximum dimension. This configuration is illustrated on enlarged plan and section drawings in the Structure Plan drawing set (in **Appendix A**).
- Footpath and building frontage on the northern side of Harbour Drive – in order to promote future active building frontage on the northern side of the street the following is recommended:
  - A six metre overall footpath width, comprising a 4 metre uninterrupted footway/shareway and a 2 metre zone behind the kerb for tree planting (via a continuous trench system similar to that proposed for the at-grade median) and street furniture – including seating and streetlights;
  - A zero building setback line, which will encourage buildings to come up to, and address, the street; and
  - Encourage small to medium sized (footprint) retail / food outlets to directly front the street, covered by a continuous (minimum 3.5metre width) awning.



#### 4.5 Precinct Traffic Calming

It is proposed to slow (calm) traffic entering the core area (Main Street) of Harbour Drive – from Camperdown Street to Orlando Street, by:

- Narrowing the road carriageway for eastbound traffic, from Edinburgh Street down the hill to Camperdown Street.
- The intersection of Camperdown and Harbour Drive is proposed to be transformed into a vehicle threshold – to aid in lowering traffic speeds and promoting general safety – whilst maintaining all of its current vehicle turning manoeuvres. To promote traffic calming through the intersection the following is proposed:
  - Harbour Drive entry and exit carriageways at the intersection into and from the Core area (Main Street) are separated and narrowed;
  - The pavement of the intersection is differentiated from normal road paving (bitumen) ie. exposed aggregate insitu concrete.
  - At-grade rumble strips are incorporated on the Harbour Drive entry and exit lanes (only).
- Reconfiguring the Harbour Drive / Orlando Street intersection – such that traffic entering from the west will be slowed significantly (to a stop in many instances) at the start of the Core area (Main Street) section. In addition, it is also proposed to introduce a special paving treatment at this intersection, similar to the intersection of Camperdown and Harbour Drive (ie. exposed aggregate insitu concrete), to promote general traffic calming.
- Shared Zone regulatory road signage be installed at both vehicle entries into the Core area (Main Street).



Example of traffic calming at intersection by changing paving treatment – Clarence Street, Port Macquarie



Example of ramped rumble strip to reduce traffic speeds – Clarence Street, Port Macquarie



#### 4.6 Broader Open Space and Pedestrian Connections

One of the key objectives of the Structure Plan is to improve pedestrian connections to and from Harbour Drive and to provide for improved public spaces in the Precinct generally. A number of initiatives are proposed, including:

- A stronger and more direct pedestrian/ cycleway connection from the Jetty and Marina Precinct to Harbour Drive over the North Coast Rail line. This can be achieved by a pedestrian overpass – starting from the end of the Jetty axis thoroughfare – and/or an improved ground level pedestrian crossing at the rail line.
- A plaza fronting the Jetty Strip and Main Street, using the northern portion of NSW Property land and the road reservation in the vicinity of the existing small car park. This plaza and pocket park will form part of an important open space connection between the Jetty Strip and the Jetty and Marina Precinct. It is proposed to provide shaded open space and seating areas in this plaza.
- Incorporation of generous footways down both sides of Harbour Drive and a continuous at-grade median in the middle of the street
- Incorporation of an on-road bike lane.
- Refurbishment of walkway and outdoor dining areas along the Jetty Strip. The existing configuration of dining platforms, retaining walls, tree planting and artworks has served this area well over many years, but it is tired and in need of attention. It is proposed to refurbish the whole of this area by lowering levels in the adjacent road and parking corridor to allow continuous pavement falls from the building line to the kerb, including removing existing steps into buildings wherever possible. This proposal offers distinct advantages in terms of improvements to pedestrian access generally, rationalising drainage and simplifying spaces. These works should adopt the same ‘fine grain’ character that currently exists. The works will comprise the following:
  - Retention of existing tree planting, where viable;
  - New pedestrian paving, with continuous (approximate 1%) fall to the kerb;
  - New outdoor dining platforms, including new paving and balustrades;
  - New shade structures – It is noted that these should be light weight and removable. No permanent shade structures or weather protection structures will be permitted.
  - New drainage and lighting;
  - New advanced tree planting; and
  - Pandanus Palms – It is noted that (subject to feasibility assessment) existing Pandanus Palms are to be transplanted within the immediate vicinity.

#### 4.7 On-Street Parking

As identified in the site analysis, there are approximately 38 existing carparking spaces in Harbour Drive from Camperdown Street to Orlando Street (including the 7 spaces in the small off-street carparking area) and approximately 33 existing on-street carparking spaces in Collingwood and Edgar Streets. A fundamental objective of the Main Street improvement works is to retain as much on-street carparking as is practically possible.

Parallel parking is proposed down both sides of Harbour Drive in the Core area (Main Street). Parallel parking has the advantage of providing a buffer between pedestrian and vehicle areas, whilst not requiring a large (width) dimension (unlike, say, angle parking). Approximately 32 Parallel spaces are provided in Harbour Drive.

Collingwood Street, between Edgar Street and Orlando Street, has a 30 metre wide road reservation. It has the potential to provide centre parking. Providing centre parking adjacent retail and commercial areas becomes an attractive option where the street does not provide a major through road function (like Orlando Street and Harbour Drive) – because this format maximizes on-street parking potential. Collingwood Street has the potential to provide approximately 32 car spaces.

Edgar Street, adjacent the Jetty Village, is considered unnecessarily wide and offers the potential for reconfiguring and boundary adjustment. If this proposal is adopted it will result in a slight reduction of parking in this street section.

With the above parking proposals, approximately 94 on- carparking spaces have been provided. This is approximately 23 more on-street carparking spaces than existing.

Additional on-street carparking potential was also identified in the following areas:

- On-street carparking in Harbour Drive, between Camperdown and Edinburgh Streets – 8 parallel parking spaces are considered possible.
- Existing on-street parking in Camperdown Street in the vicinity of the High School could be formalised (by line marking). This may increase efficiency and parking space numbers.
- Partial use of the NSW Property land parcel for off-street carparking. This has been identified as a priority action through the engagement process.

#### 4.8 Activation through Development

There is potential for collaboration between Council and neighbouring property owners to deliver significant benefits for the Main Street and neighbouring streets, and the public realm generally. These are:

- The potential for road closure and property boundary adjustment to provide a uniform road corridor (width) for Harbour Drive, and make available, on balance, more land for mixed-use residential and commercial development.
- The potential to significantly increase active commercial and retail building frontage to Harbour Drive, Orlando Street and Collingwood Street.



## 5. ENGAGEMENT

### 5.1 Effective Engagement

Effective consultation with a wide range of stakeholders is critical to realising the full potential of this project.

An integral component of the early concept design process has been negotiations with the proponents of three (3) key development sites on the northern side of Harbour Drive and the western side of Orlando Street, with a view to unlocking development potential and activating street frontage – that benefits the landholders, other business interests in the Jetty Strip and the Coffs Harbour community.

Other key stakeholders that have been or will be consulted as part of the project include:

- Landowners and their representatives:
- The Pier Hotel;
- The Department of Education;
- Council (Jetty Theatre and Road Reserve);
- NSW State Government Jetty Foreshores Project Control Group;
- Residents and Landowners within and adjacent to the study area.
- Community:
- Residents;
- Visitors;
- School students from Coffs Harbour High School.
- Council:
- Councillors;
- Council's internal Working Group
- Other internal staff.

### 5.2 The Engagement Process

The engagement process undertaken as part of the project is summarised as follows:

1. Conduct individual meetings with key developers, landowners, businesses and council's Internal Working Group at the outset of project. *(Complete)*
2. Once preliminary designs have been prepared, conduct two (2) full day design charrettes with the following:
  - i. Council staff and representatives of State Government agencies (13 February 2020) *(Complete)*;
  - ii. Other stakeholders, including representatives of state government agencies, other landholders or their representatives, Councillors and council staff (14 February 2020) *(Complete)*;
3. Following the preparation of Draft Concept Plans make presentations to Council's Working Group and Steering Group and key affected landholders. Due to the restrictions on gatherings as part of community response to CoVID-19, this direct engagement was not possible. To enable further stakeholder feedback the Draft Concept Plans were forwarded to the participants of the design charrettes (as part of 2. above) – including plans, a 3D fly-through model and a draft Structure Plan report for their review and comment.
4. Following the development of Final Draft Plans and their review by Council, place the plans on Public Exhibition as per Council's Community Participation and Engagement Plan.

### 5.3 Results of Design Charrettes

Full day Design Charrettes with (i) Council staff and Councillors, and (ii) Other Stakeholders, were concluded on the 13 and 14 February 2020, respectively.

The workshop agendas included technical and stakeholder briefings, the presentation of preliminary plans by the Consultant Team and design workshops.

The general feedback following the Design Charrettes is that they were considered worthwhile by all participants.



A summary of workshop outcomes for each of the Design Charrettes is as follows:

## Design Charrette (i) Workshop Outcomes:

- Provide improved crossing at Jetty Theatre intersection;
- Ensure that trees are included within the upgrade works;
- Consider drainage/median details carefully to support the median trees;
- General acceptance of the continuous at-grade median;
- Concerns were expressed in relation to pedestrians crossing in the vicinity of the Pier Hotel – this needs to be addressed;
- Raised mid-block crossing – possibly 75mm height;
- Generally supportive of the main intersection configuration;
- Bus Stop on northern side of Harbour Drive is problematic – review location;
- Investigate alternative site for Bus Stop within Orlando Street – but some difficulties with this;
- Difficulties with road grade-separation at railway crossing;
- Pedestrian overpass should be considered in terms of recognition of importance of pedestrian linkage;
- Project needs to deliver structure to frame negotiations with Developers – particularly if road closures are desired;
- Support for term “Structural Masterplan”;
- General support of Collingwood Street car parking – subject to Developer take up;
- Next step is to finalise dimensional properties relating to reconfiguration of Harbour Drive;
- Some difference of opinion on on-road versus off-road cycleway provision.

## Design Charrette (ii) Workshop Outcomes [as different from outcomes of Design Charrette (i)]:

- General concept supported;
- Pedestrian crossing needs to be incorporated in intersection outside Hotel;
- Consider intersection of Hood Street /Harbour Drive as change of pavement to act as traffic calming device;
- Consider more angle parking in Harbour Drive west of Hotel and in Camperdown Street;
- Consider relocating bus stop (currently near Hogs Breath);
- Consider slowing traffic before Edinburgh Street intersection, i.e., treatment of this intersection similar to Hood Street;
- Closer look at pedestrian (for school) access down laneway, and particularly how it gets across to railway crossing.







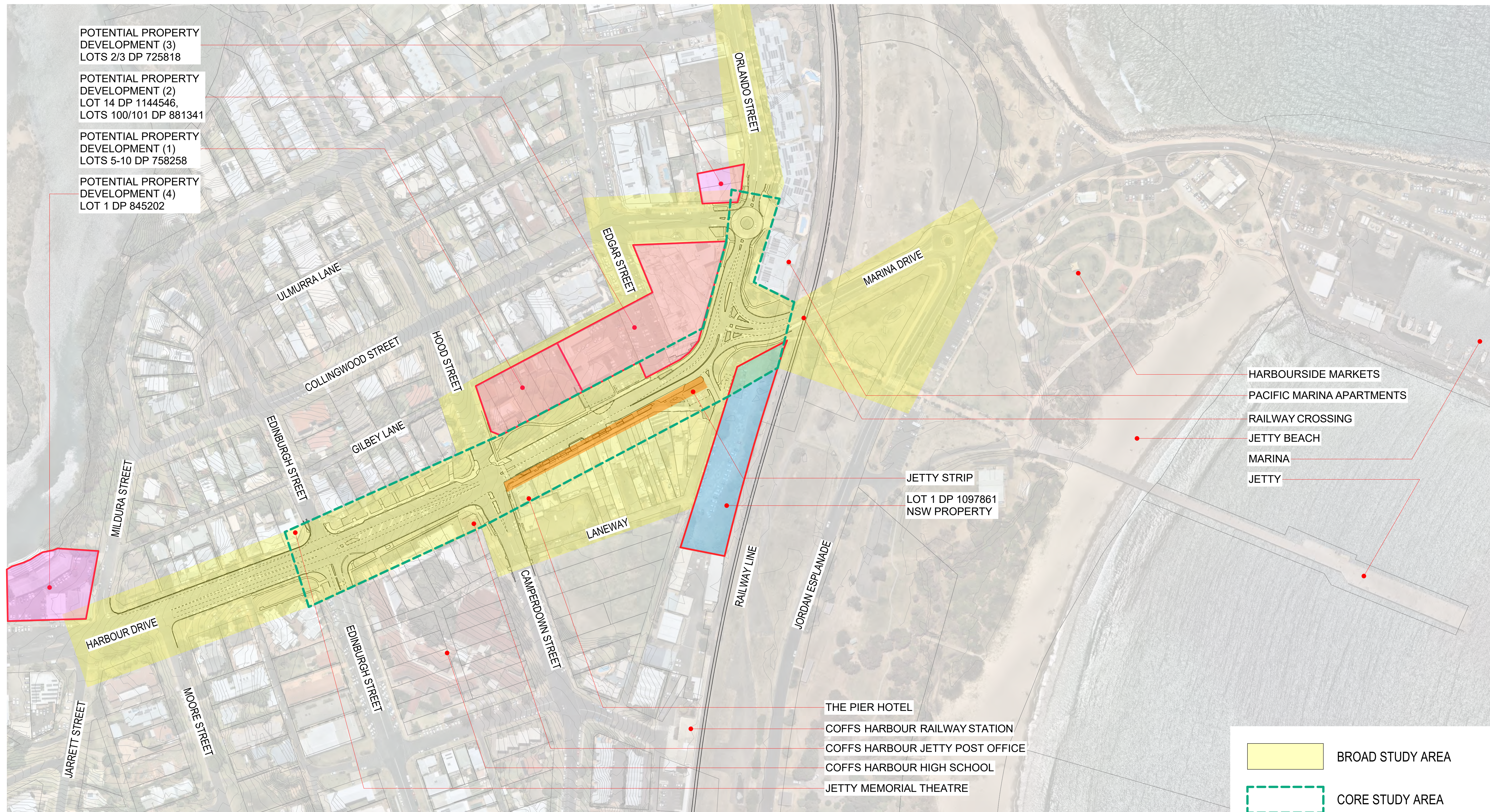


DRAWING INDEX

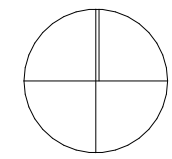
SHEET 1	STUDY AREA PLAN + POTENTIAL PROPERTY DEVELOPMENT
SHEET 2	STRUCTURE PLAN 1
SHEET 3	STRUCTURE PLAN 2
SHEET 4	TYPICAL HARBOUR DRIVE CROSS SECTION IN CORE AREA
SHEET 5	BOULEVARD DETAIL PLAN
SHEET 6	BESPOKE STREET FURNITURE OPTIONS
SHEET 7	STREET FURNITURE + PAVING OPTIONS
SHEET 8	STREET TREE OPTIONS
SHEET 9	STAGING PLAN
SHEET 10	DEMOLITION OF EXISTING SERVICES
SHEET 11	PROPOSED SERVICES
SHEET 12	POTENTIAL DEVELOPMENT PLAN
SHEET 13	PERSPECTIVES
SHEET 14	PERSPECTIVES
SHEET 15	PERSPECTIVES

STRUCTURE PLAN





SCALE: 1:1500 @ A1  
1:3000 @ A3



## STUDY AREA PLAN + POTENTIAL PROPERTY DEVELOPMENT

### COFFS JETTY STRIP STRUCTURE PLAN

MAY 2020  
SHEET 1 OF 15



LEGEND

EXISTING

PROPERTY BOUNDARIES

PROPOSED

ROAD PAVEMENT

ROAD MEDIAN

VEHICLE RUMBLE STRIP

SPECIAL FINISH CONCRETE ROAD PAVEMENT

PEDESTRIAN FOOTPATH / SHAREWAY

VEHICLE DRIVEWAYS/ CROSSOVERS

PERMEABLE PAVING  
- WITH STRUCTURAL ROOT CELLS UNDER

OUTDOOR DINING PLATFORM

GARDEN BED

TURF

SMALL TO MEDIUM TREE  
REFER TO PLANT SCHEDULE ON SHEET 8

MEDIUM TO LARGE TREE  
REFER TO PLANT SCHEDULE ON SHEET 8

PLANTER BOLLARDS

INDICATIVE STREET LIGHT LOCATIONS

POTENTIAL PROPERTY ADJUSTMENT

PUBLIC ART IS TO BE DESIGNED IN ACCORDANCE WITH COUNCIL'S PUBLIC ART POLICY (POL-028) TO THE VALUE OF 1% OF THE PROJECT BUDGET FOR STAGES 1, 1A AND 2. ARTWORK IS NOT TO BE DESIGNED OR INSTALLED UNTIL COMPLETION OF THE CONSTRUCTION OF STAGES 1, 1A AND 2, SO AS TO ENSURE A COMPREHENSIVE UNDERSTANDING OF THE STREETScape AND HOW IT FUNCTIONS IN RELATION TO LIGHTING AND VISUAL CORRIDORS.

AT-GRADE MEDIAN TO FACILITATE TRAFFIC CALMING, PEDESTRIAN CROSSING AND TREE PLANTING - REFER TO DETAIL

MEDIUM TO LARGE TREE PLANTING - TREE ROOT DEVELOPMENT ENABLED WITH PERMEABLE PAVING AND STRUCTURAL ROOT CELLS

MEDIAN TREE PLANTING - TREE ROOT DEVELOPMENT ENABLED WITH PERMEABLE PAVING AND STRUCTURAL ROOT CELLS

MEDIAN TREE PLANTING SPECIES FLINDERSIA AUSTRALIS

IN CONJUNCTION WITH THE FUTURE DEVELOPMENT OF THE JETTY VILLAGE AND NSW PUBLIC WORKS SITES, INVESTIGATE THE POTENTIAL RECONFIGURATION OF EDGAR STREET TO PARALLEL PARKING TO REDUCE THE ROAD RESERVATION WIDTH

IN CONJUNCTION WITH THE FUTURE DEVELOPMENT OF THE JETTY VILLAGE SITE, INVESTIGATE THE POTENTIAL RECONFIGURATION OF COLLINGWOOD STREET FOR CENTRE PARKING

EXISTING TREES

PARALLEL PARKING

ORLANDO STREET / HARBOUR DRIVE / MARINA DRIVE - RECONFIGURATION OF INTERSECTION AND INCLUSIONS OF SERVICES / CONDUITS TO PROVIDE FOR FUTURE SIGNALISATION

IMPROVE AT-GRADE PEDESTRIAN ACCESS AT RAIL CROSSING INVESTIGATE POTENTIAL FOR PEDESTRIAN OVERPASS

COFFS JETTY 'PLAZA' THIS AREA WILL INVOLVE CLOSE NEGOTIATIONS WITH NSW PROPERTY

OPEN GRASSED SPACE

SHARED VEHICLE / PEDESTRIAN SPACE

RAISED SEATING WALL / PLANTER BED

POTENTIAL CARPARK. THIS WILL INVOLVE CLOSE NEGOTIATION WITH NSW PROPERTY

REFER TO HARBOUR DRIVE - TYPICAL 'BOULEVARD' TREATMENT

RAISED MARKED CROSSING IN SAME POSITION (APPROXIMATELY) AS EXISTING MARKED CROSSING

3 METRE WIDE VEHICLE RUMBLE STRIP RAMP TO SLOW TRAFFIC

NEW OUTDOOR DINING PLATFORM - REFER TO DETAIL

SMALL TO MEDIUM TREE PLANTING - TREE ROOT DEVELOPMENT ENABLED WITH PERMEABLE PAVING AND STRUCTURAL ROOT CELLS

INVESTIGATE FORMALISING (LINE MARKING) IN CAMPERDOWN STREET TO MAXIMISE CAR SPACES

INVESTIGATE POTENTIAL FOR WALKWAY ALONG JETTY LANE. THIS WILL INVOLVE NEGOTIATION WITH THE NEIGHBOURING LANDHOLDER, NSW DEPARTMENT OF EDUCATION

SCALE: 1:500 @ A1  
1:1000 @ A3

KING + CAMPBELL

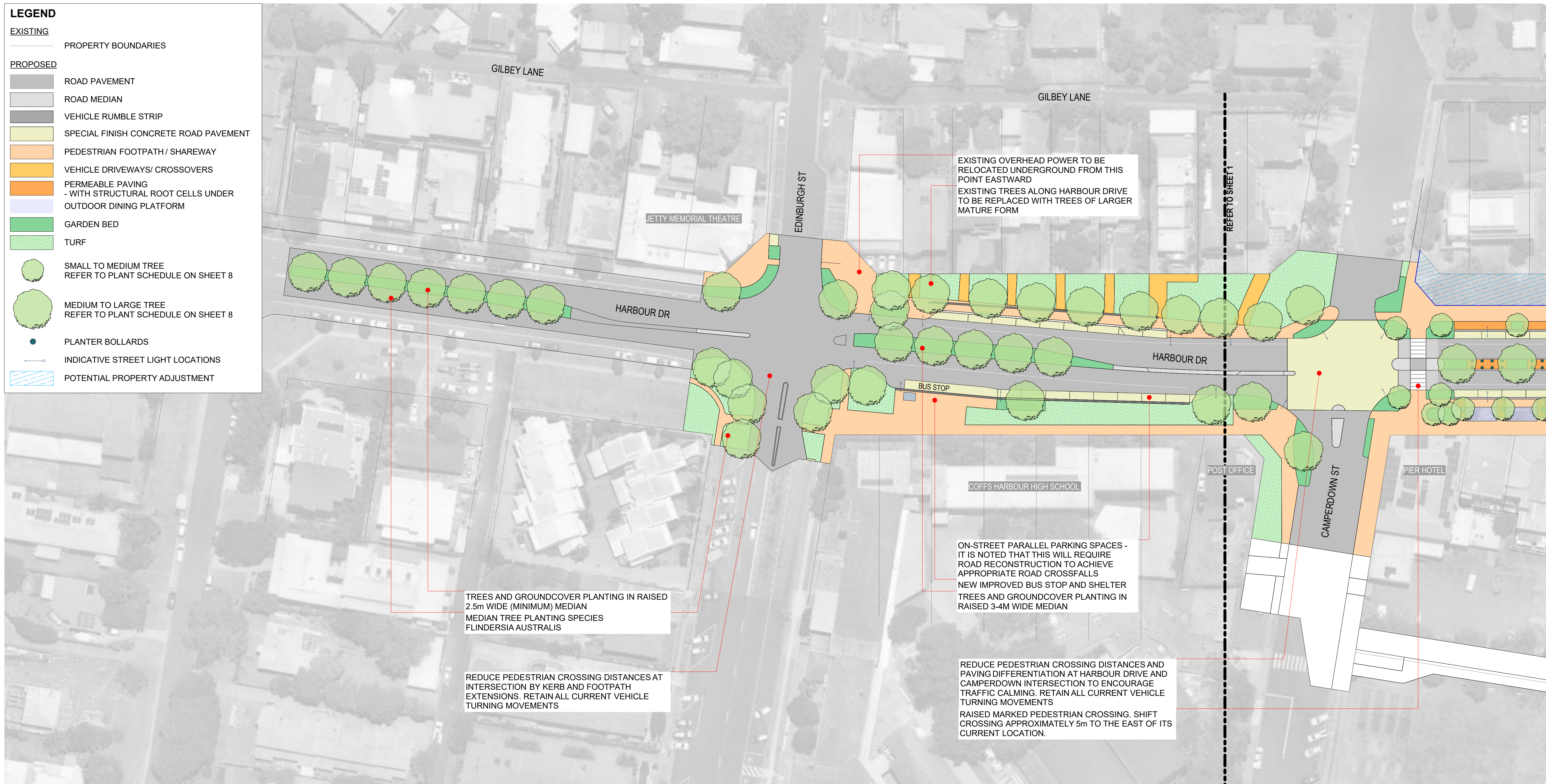
STRUCTURE PLAN - SHEET 1

COFFS JETTY STRIP  
STRUCTURE PLAN

MAY 2020  
SHEET 2 OF 15

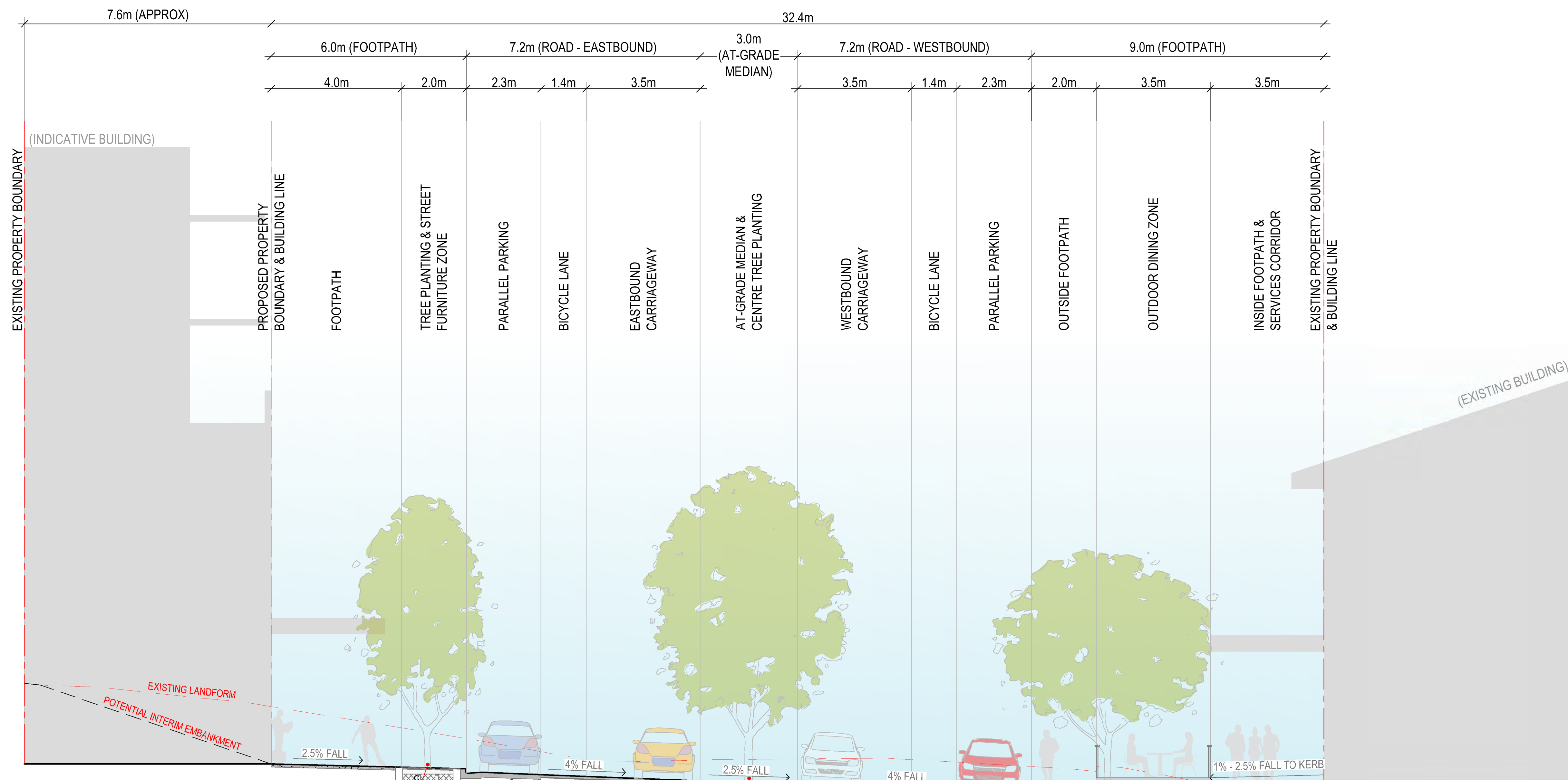
STRUCTURE PLAN





STRUCTURE PLAN





- POSSIBLE RELOCATED SEWER MAIN
- LOCATION OF EXISTING SEWER MAIN
- MEDIUM TREE PLANTING
  - TRISTANIOPSIS LAURINA (WATERGUM)
- CONTINUOUS TRENCH & STRUCTURAL ROOT CELLS FOR TREE ROOT DEVELOPMENT
- NEW STORMWATER DRAINAGE
- SPECIAL FINISH (EXPOSED AGGREGATE) CONCRETE TO DELINEATE PARKING AREAS
- APPROXIMATE LOCATION OF EXISTING STORMWATER
- BITUMEN CARRIAGEWAY AND MANOUVERING LANE
- CONTINUOUS TRENCH & STRUCTURAL ROOT CELLS FOR TREE ROOT DEVELOPMENT
- MEDIUM TO LARGE TREE PLANTING
  - FLINDERSIA AUSTRALIS (NATIVE TEAK)

- NEW STORMWATER DRAINAGE
- TREE PLANTER (IN STRUCTURAL ROOT CELLS) - RECOMMENDED SPECIES:
  - PANDANUS PEDUNCULATUS (COASTAL SCREW PINE)
  - TRISTANIOPSIS LAURINA (WATERGUM)

- STRUCTURAL ROOT CELL (VAULT) FOR TREE ROOT DEVELOPMENT
- OUTDOOR DINING PLATFORMS
- APPROXIMATE LOCATION OF EXISTING STORMWATER
- 3.5m WIDE FOOTPATH/ SHAREWAY

NOTE: THE CADASTRAL BOUNDARY INFORMATION DEPICTED ON THIS PLAN HAS BEEN DETERMINED BY DEED ONLY. FOR THIS PARTICULAR SURVEY THE ACCURACY OF THE CADASTRAL BOUNDARY POSITION IS EXPECTED TO BE +/- 0.1m.

STRUCTURE PLAN

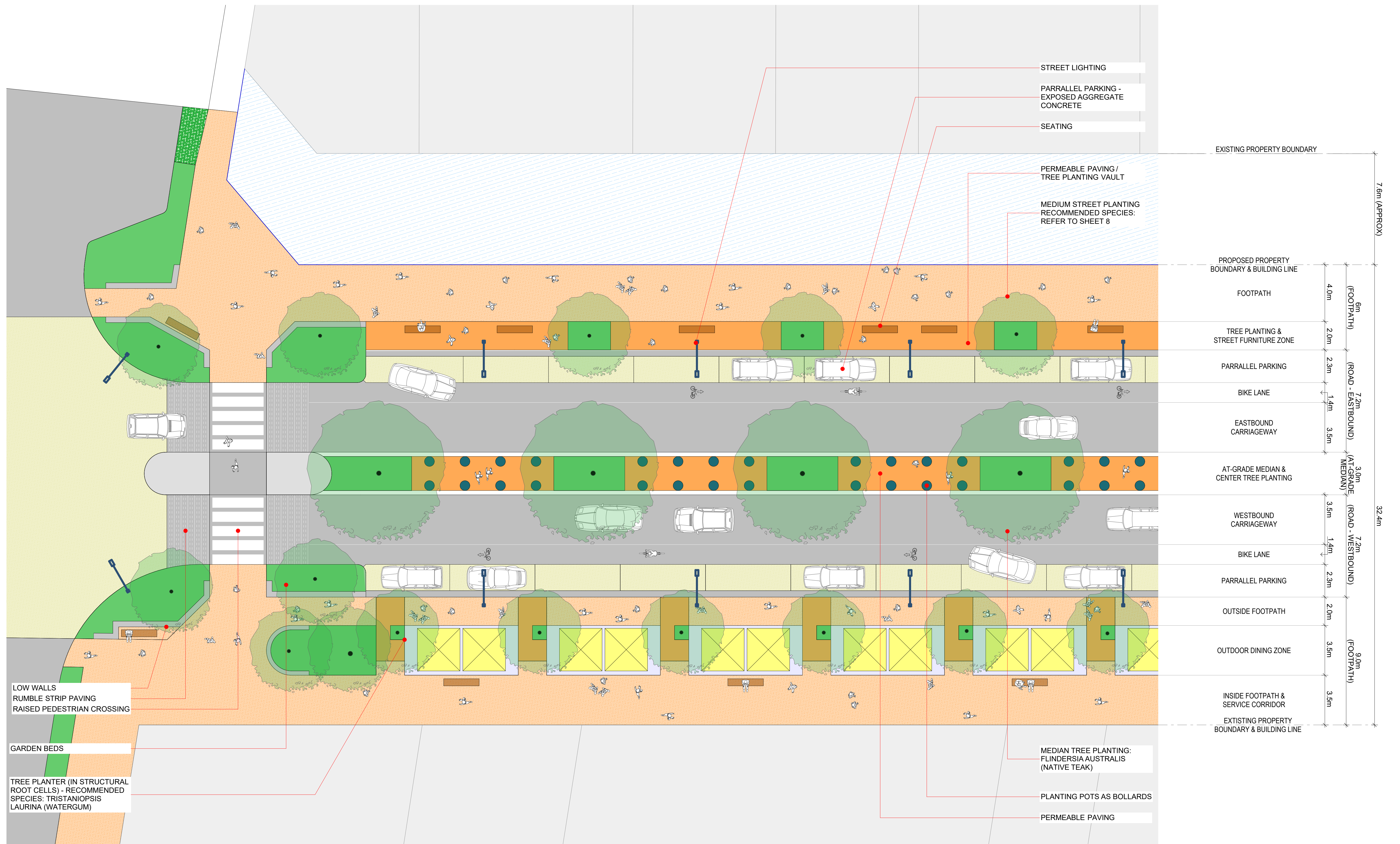
TYPICAL HARBOUR DRIVE CROSS SECTION IN CORE AREA

## COFFS JETTY STRIP STRUCTURE PLAN

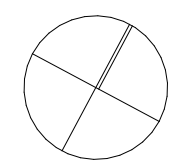
MAY 2020

SHEET 4 OF 15





SCALE: 1:125 @ A1  
1:250 @ A3



## BOULEVARD DETAIL PLAN

### COFFS JETTY STRIP STRUCTURE PLAN

MAY 2020

SHEET 5 OF 15





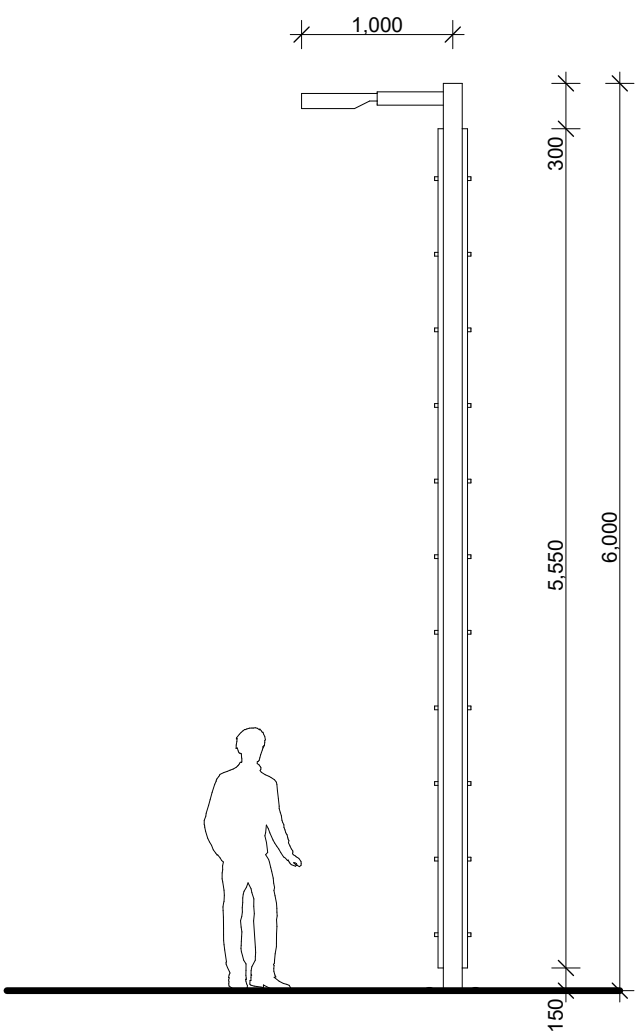
MATERIAL & INSPIRATION REFERENCE - COFFS HARBOUR JETTY  
WEATHERED LARGE SECTIONED TIMBER AND STEEL  
IMAGE SOURCE: COFFS COAST HOLIDAY PARKS



TIMBER FOLDED SEAT



PEDESTRIAN SCALE LIGHTS



TIMBER 120L OR 240L BIN ENCLOSURE



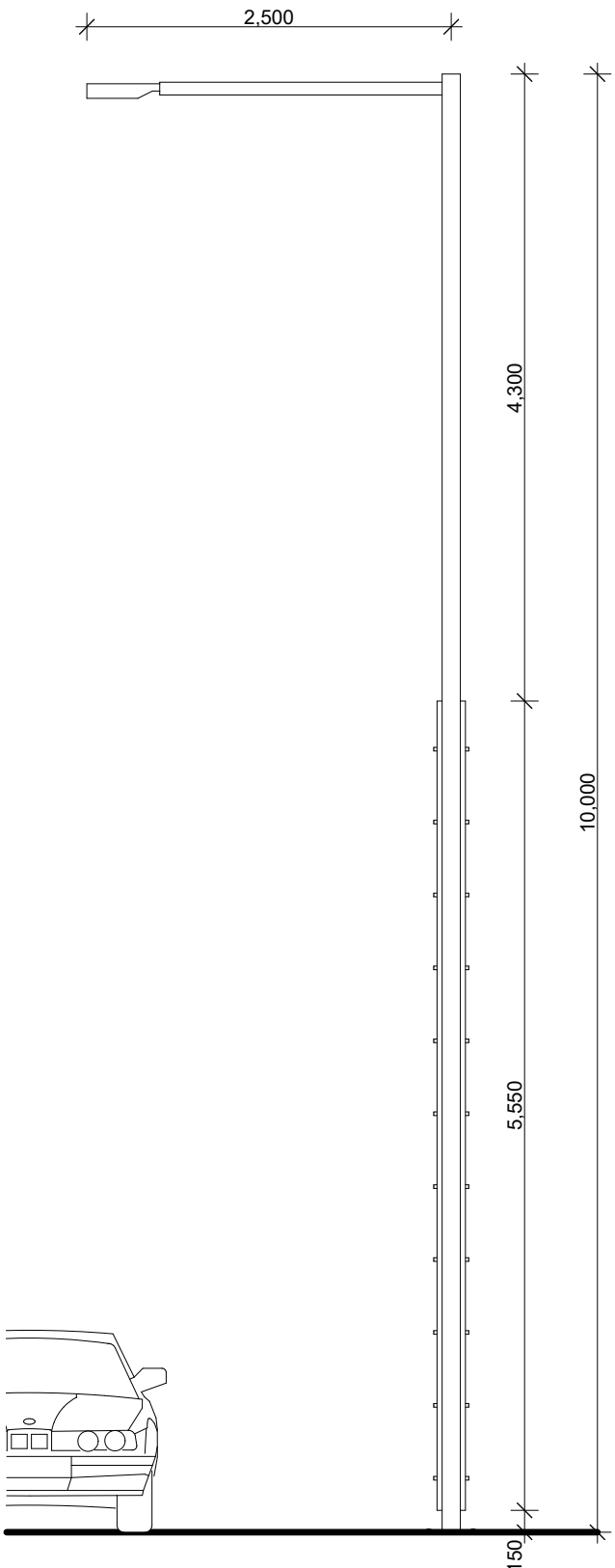
TIMBER BILLET SEAT



TIMBER FOLDED SEAT WITH BACKREST



STREET LIGHTS



STRUCTURE PLAN



TIMBER 240L RECYCLING BIN ENCLOSURE SET



TIMBER BILLET SEAT WITH BACKREST



TIMBER FOLDED SEAT WITH BACKREST

## BESPOKE STREET FURNITURE OPTIONS

### COFFS JETTY STRIP STRUCTURE PLAN

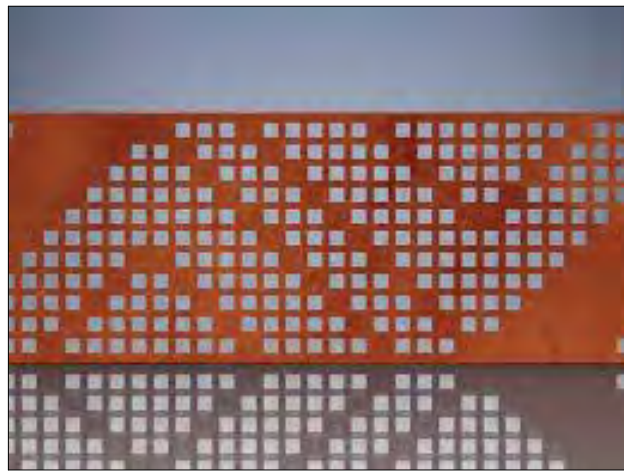
MAY 2020

SHEET 6 OF 15





BIKE RACK - WHEEL STOP CO - BAY CITY RACK  
CAST ALUMINIUM



POTENTIAL USE OF CORTEN STEEL - EDGING, PLANTERS,  
CAFE SCREENS, ARTWORKS



LARGE POTS AS BOLLARDS - QUATTRO U POT  
COMBINATION GLASS FIBRE - REINFORCED  
CONCRETE SCULPTURED FORM



EXAMPLE OF AT-GRADE MEDIAN TREATMENT (INVERELL, NSW) - INCLUDING STRUCTURAL TREE CELLS, TREE ROOT PLANTING VAULTS,  
PERMEABLE PAVING & ADVANCED TREE PLANTING (NOTE: SPECIES IN PHOTO NOT THE SAME AS PROPOSED)



PEDESTRIAN PAVING -  
RECTANGULAR CONCRETE PAVERS "PLANK" FORMAT



RUMBLE STRIP PAVING - BLUESTONE PAVERS



SPECIAL VEHICLE PAVING AREAS (THRESHOLDS) -  
EXPOSED AGGREGATE CONCRETE



STREET DRAIN - PATTERNED METAL GRATES WITHIN  
"PLANK" FORMAT PAVERS

STRUCTURE PLAN

# STREET FURNITURE + PAVING OPTIONS

## COFFS JETTY STRIP STRUCTURE PLAN

MAY 2020

SHEET 7 OF 15



THE MEDIAN TREE



**FLINDERSIA australis**  
"Australian Teak" or "Crows Ash"  
HEIGHT: 10-15m WIDTH: 6-8m

NOTE: WITH THE EXCEPTION OF PANDANUS  
PENDUNCULATUS, WHICH MAY BE TRANSPLANTED FROM  
EXISTING SITUATIONS (SUBJECT TO FEASIBILITY  
ASSESSMENT), ALL OTHER TREES SHOULD BE SOURCED  
FROM REPUTABLE ADVANCED TREE SUPPLIERS AND MUST  
BE A CERTIFIED NATSPEC GROWN TREE.

ALL TREES SHOULD BE A MINIMUM 400L AT THE TIME OF  
PLANTING. MEDIAN TREES SHOULD BE 800L AT THE TIME OF  
PLANTING.

POTENTIAL SUPPLIER: TREES IMPACT.

OPTIONS FOR PARKS



**CUPANIOPSIS anacardioides**  
"Tuckeroo"  
HEIGHT: 8-15m WIDTH: 3-5m



**ELAEOCARPUS eumundii**  
"Quandong"  
HEIGHT: 7-10m WIDTH: 2-6m



**WATERHOUSEA floribunda**  
"Weeping lily pilli"  
HEIGHT: 10m WIDTH: 8m

OPTIONS FOR EDGE OF ROAD PLANTING AND PARKS



**TRISTANIOPSIS laurina**  
"Water gum"  
HEIGHT: 5-15m WIDTH: 3m



**WATERHOUSEA floribunda**  
"Weeping lily pilli"  
HEIGHT: 10m WIDTH: 8m



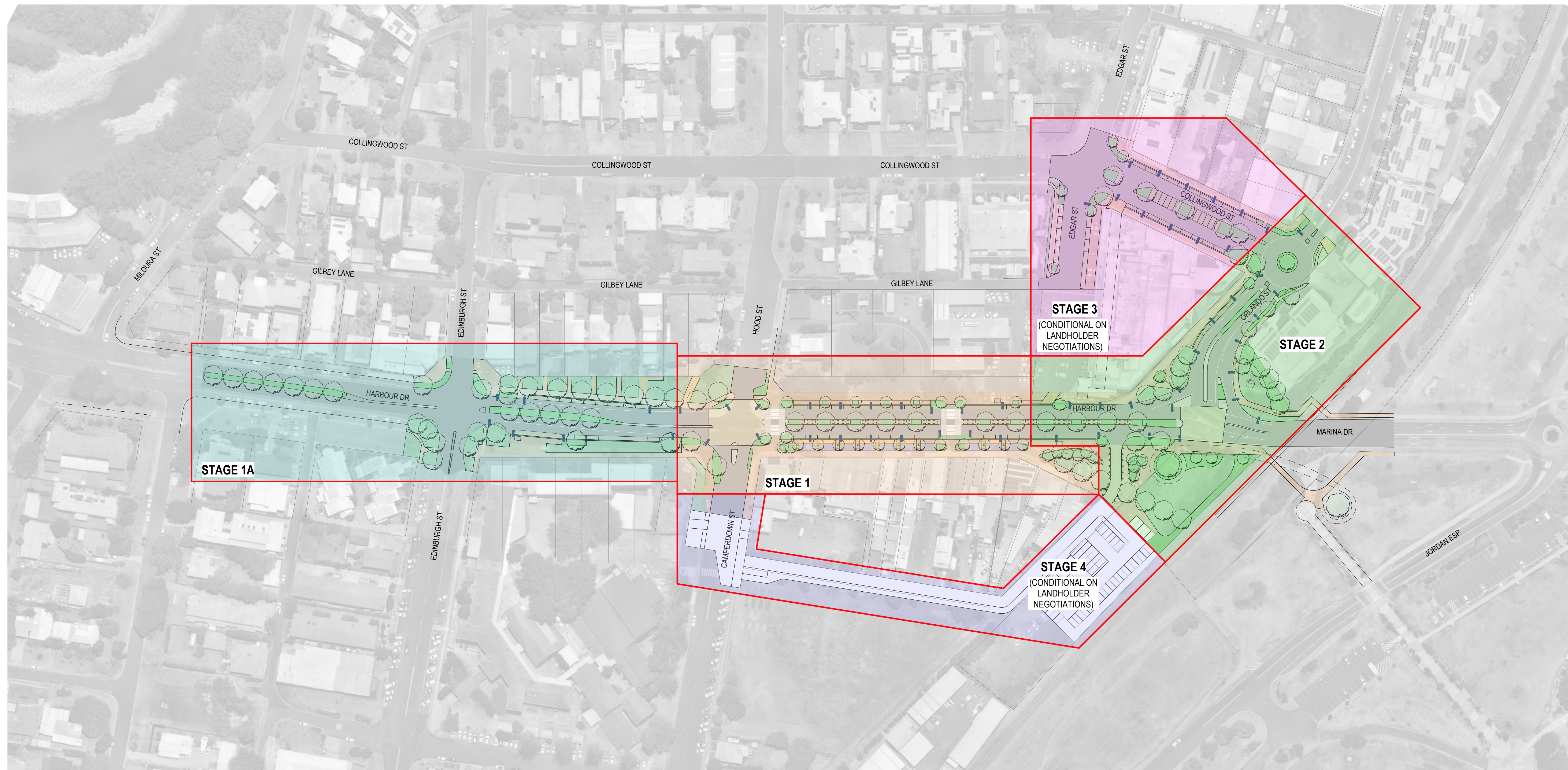
**AGATHIS robusta**  
"Queensland Kauri Pine"  
HEIGHT: 12-15m WIDTH: 6m



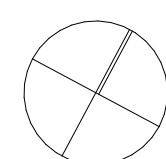
**PANDANUS pedunculatus**  
"Pandanus palm"  
HEIGHT: 4-5m WIDTH: 2-3m (transplanted if feasible)

STRUCTURE PLAN





SCALE: 1:1000 @ A1  
1:2000 @ A3





**LEGEND**

EXISTING SERVICES TO BE DEMOLISHED

COMMUNICATION

WATER

UNDERGROUND POWER

OVERHEAD POWER

STORMWATER

OPTIC

SEWER

EXISTING SERVICES TO BE RETAINED

COMMUNICATION

WATER

UNDERGROUND POWER

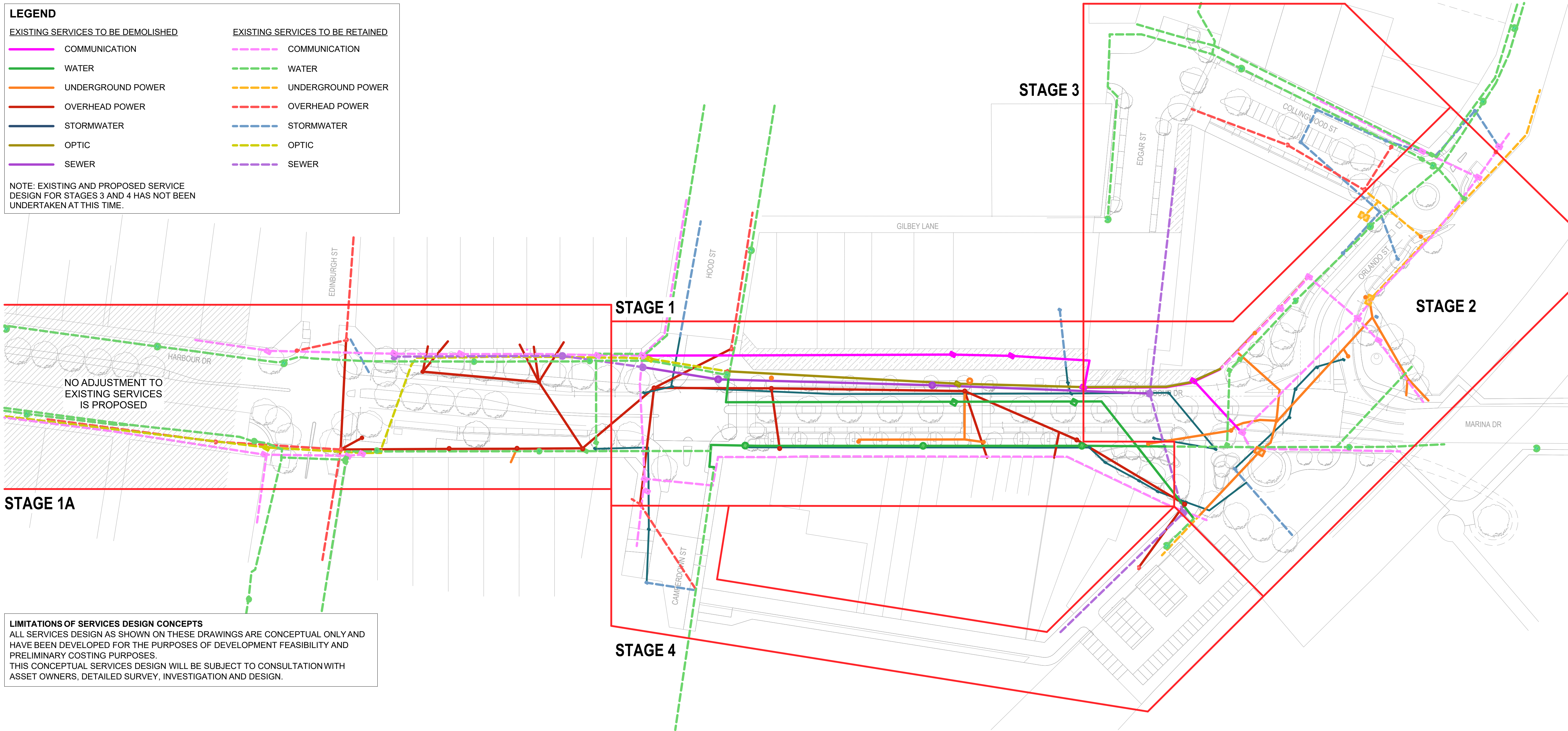
OVERHEAD POWER

STORMWATER

OPTIC

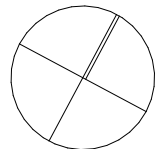
SEWER

NOTE: EXISTING AND PROPOSED SERVICE DESIGN FOR STAGES 3 AND 4 HAS NOT BEEN UNDERTAKEN AT THIS TIME.



**LIMITATIONS OF SERVICES DESIGN CONCEPTS**  
ALL SERVICES DESIGN AS SHOWN ON THESE DRAWINGS ARE CONCEPTUAL ONLY AND HAVE BEEN DEVELOPED FOR THE PURPOSES OF DEVELOPMENT FEASIBILITY AND PRELIMINARY COSTING PURPOSES.  
THIS CONCEPTUAL SERVICES DESIGN WILL BE SUBJECT TO CONSULTATION WITH ASSET OWNERS, DETAILED SURVEY, INVESTIGATION AND DESIGN.

SCALE: 1:750 @ A1  
1:1500 @ A3





**LEGEND**

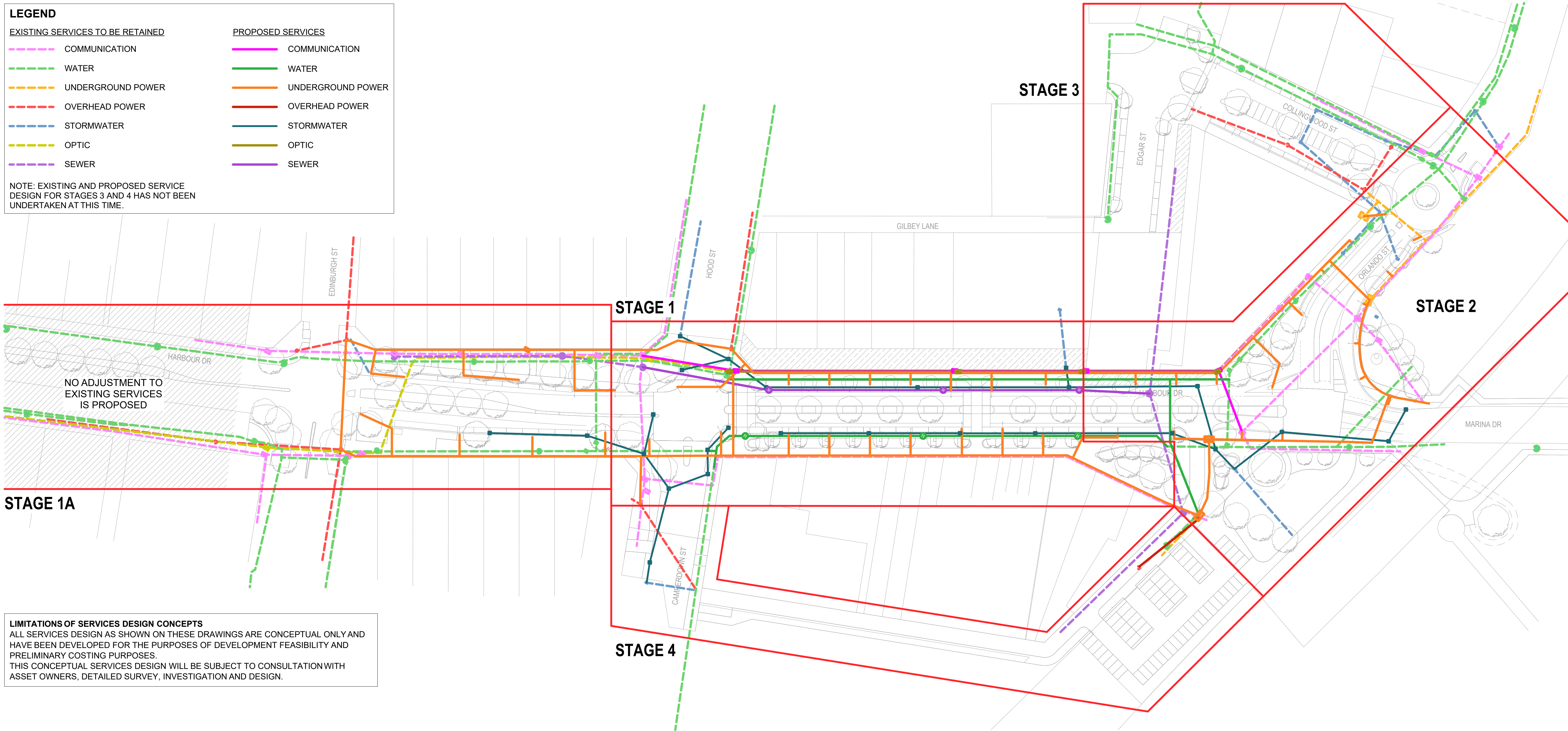
**EXISTING SERVICES TO BE RETAINED**

- COMMUNICATION
- WATER
- UNDERGROUND POWER
- OVERHEAD POWER
- STORMWATER
- OPTIC
- SEWER

**PROPOSED SERVICES**

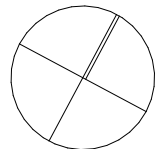
- COMMUNICATION
- WATER
- UNDERGROUND POWER
- OVERHEAD POWER
- STORMWATER
- OPTIC
- SEWER

NOTE: EXISTING AND PROPOSED SERVICE DESIGN FOR STAGES 3 AND 4 HAS NOT BEEN UNDERTAKEN AT THIS TIME.

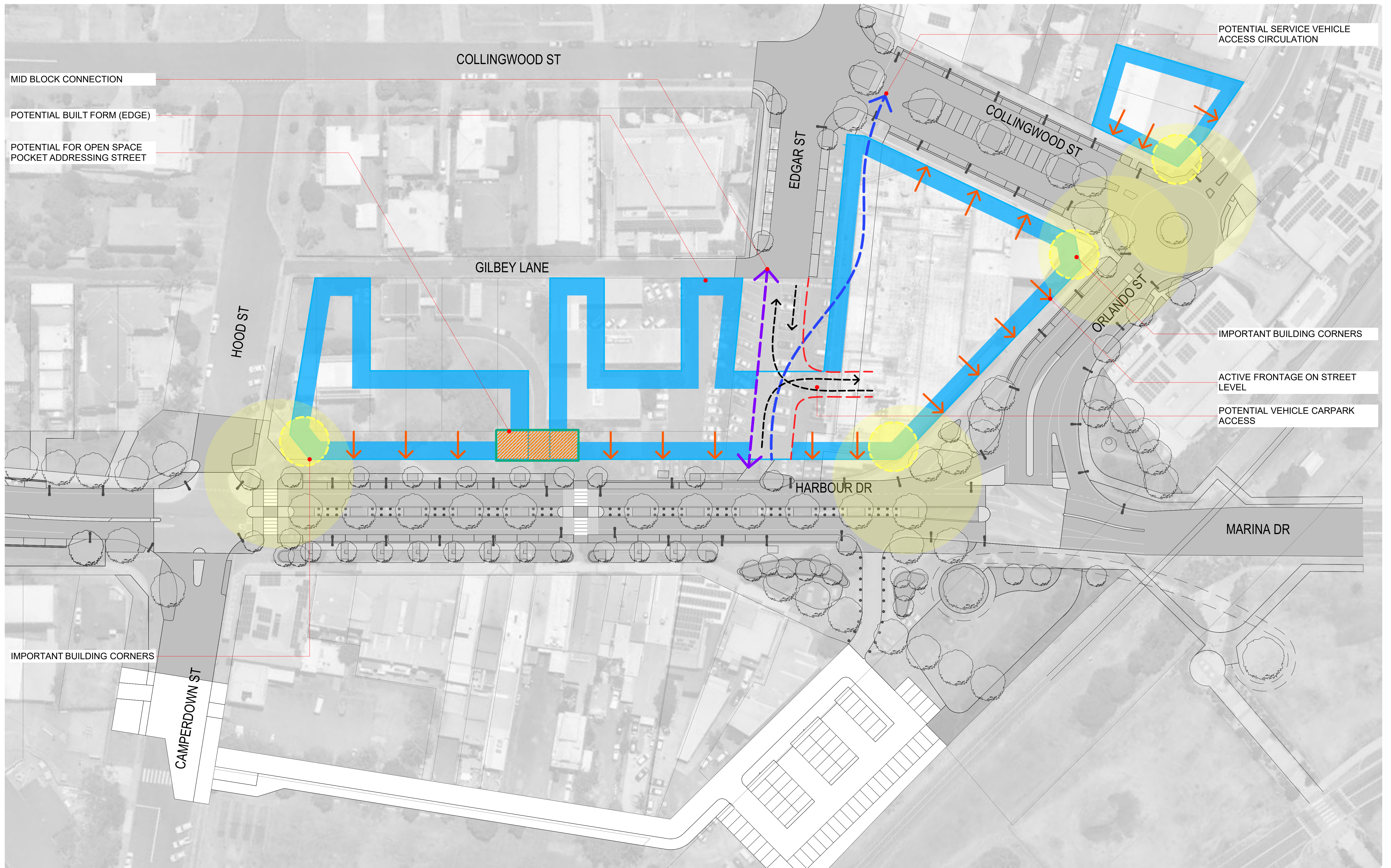


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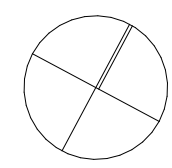
SCALE: 1:750 @ A1  
 1:1500 @ A3







SCALE: 1:500 @ A1  
1:1000 @ A3



## POTENTIAL DEVELOPMENT PLAN

### COFFS JETTY STRIP STRUCTURE PLAN

MAY 2020

SHEET 12 OF 15

KING + CAMPBELL



STRUCTURE PLAN





THE AT-GRADE MEDIAN AS AN INTEGRAL COMPONENT OF THE BOULEVARD



THE 'BOULEVARD'



RECONFIGURED INTERSECTION OF HARBOUR DRIVE / MARINA DRIVE / ORLANDO STREET, LOOKING BACK TO PLAZA



RECONFIGURED INTERSECTION OF HARBOUR DRIVE / MARINA DRIVE / ORLANDO STREET, LOOKING UP BOULEVARD

STRUCTURE PLAN





NEW RAISED PEDESTRIAN CROSSING IN APPROXIMATELY THE SAME MID-BLOCK LOCATION AS THE EXISTING



NEW RAISED MID-BLOCK PEDESTRIAN CROSSING AND WIDE SOUTHERN FOOTPATH INCORPORATING NEW DINING PLATFORMS



NEW RAISED PEDESTRIAN CROSSING IN FRONT OF PIER HOTEL - ITS LOCATION HAS BEEN MOVED APPROXIMATELY 4-5 METRES TO THE EAST OF THE CURRENT CROSSING LOCATION



NEW RAISED PEDESTRIAN CROSSING AT PIER HOTEL, LOOKING EAST

STRUCTURE PLAN





OUTDOOR DINING PLATFORMS AND PUBLIC SEATING



POTENTIAL TO TRANSPLANT EXISTING TREES (PANDANUS PALMS) INTO NEW PLANTING LOCATIONS



VIEW DOWN SOUTHERN SIDE OF HARBOUR DRIVE BOULEVARD FROM NEAR CAMPERDOWN STREET INTERSECTION



LARGE SECTIONED TIMBER STREET FURNITURE, PAYING TRIBUTE TO THE COFFS JETTY STRUCTURE

STRUCTURE PLAN